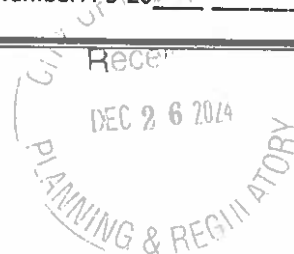


City of Worcester Planning Board



DEFINITIVE SITE PLAN APPLICATION

Division of Planning & Regulatory Services

City Hall, 455 Main Street, Room 404, Worcester, MA 01608

Phone: (508) 799-1400 x 31440 – Fax: (508) 799-1406 – E-mail: planning@worchesterma.gov (preferred)

1. PROPERTY INFORMATION

- a. 120 Washington Street (Lot 3C)
Address(es) – please list all addresses the subject property is known by
- b. 05-005-0003C
Parcel ID or Map-Block-Lot (MBL) Number
- c. Worcester District Registry of Deeds, Book 66967 Page 244
Current Owner(s) Recorded Deed/Title Reference(s)
- d. Business, General (BG-6.0), Commercial Corridors Overlay District - Downtown (CCOD-D) and Downtown/Blackstone Canal Sign Overlay District (DSOD)
Zoning District and all Zoning Overlay Districts (if any)

2. APPLICANT INFORMATION

- a. Rossi Development LLC
Name(s)
- b. 345 Boylston Street, Suite 300-5, Newton, MA 02459
Mailing Address(es)
- c. c/o Mark A. Borenstein, mborenstein@princelobel.com and (508) 688-9136
Email and Phone Number(s)
- d. Developer
Interest in Property (e.g., Lessee, Purchaser, etc.)

I certify that I am requesting the Worcester Planning Board grant the Definitive Site Plan as described below

By: Rossi Development LLC
Anthony Rossi, manager
Signer ID: L4W30QBH13

(Signature) Name: Anthony Rossi Title: Manager

3. OWNER OF RECORD INFORMATION (IF DIFFERENT FROM APPLICANT)

- a. Boston Capital Development, LLC
Name(s)
- b. 11 Beacon Street, Suite 325, Boston, MA 02108
Mailing Address(es)
- d. c/o Mark A. Borenstein, mborenstein@princelobel.com and (508) 688-9136
Email and Phone Number

Division of Planning & Regulatory Services
455 Main St., 4th Floor, Worcester, MA 01608
Office 508-799-1400 – Fax 508-799-1406

Planning@worchesterma.gov

4. REPRESENTATIVE INFORMATION

a. Mark A. Borenstein, Prince Lobel Tye LLP

Name(s)

b.

Signature(s)

c. 120 Front Street, Suite 830, Worcester, MA 01608

Mailing Address(es)

d. mborenstein@princelobel.com (508) 688-9136

Email and Phone Number

e. Attorney

Relation to Project (Architect/Attorney/Engineer/Contractor, etc.)

5. AUTHORIZATION Richard D. Mazzocchi, Authorized Signatory of
Authorization I, Boston Capital Development, LLC, Owner of Record of the property listed with the

Assessing Division of the City of Worcester, Massachusetts as Map 05 Block 005 Lot(s) 0003C, do hereby

authorize Rossi Development LLC to file this application with the Division of Planning & Regulatory

Services of the City of Worcester on this the 12th day of December, 20 24.

By: Richard D. Mazzocchi
Richard D. Mazzocchi, Authorized Signatory

On this 12th day of December, 20 24, before me personally appeared

Richard D. Mazzocchi, to me known to be the person described in and who executed the foregoing

instrument and acknowledged that they executed the same as their free act and deed.

Mark A. Borenstein

NOTARY PUBLIC

My Commission Expires: 12/1/2028



(If there is more than one owner of the land to be considered in this application, a notarized authorization is required for each owner.)

6. PROVIDE THE FOLLOWING ITEMS, 1 DIGITAL COPY IN PDF FORMAT VIA EMAIL TO PLANNING@WORCESTERMA.GOV AND CONFIRM WITH STAFF BEFORE SUBMISSION OF 1 PHYSICAL COPY BY HAND DELIVERY OR MAIL:

- Zoning Determination Form** obtained from the Inspectional Services Division (email inspections@worcesterma.gov or call 508 – 799 – 1198 for more information)
- Completed Site Plan Application, signed by all parties involved.
- Completed Tax Certification for the Applicant and Owner (if different) are attached (page 4)
- If the applicant is NOT the Owner, the **Owner(s) Authorization** for the applicant to apply is attached (page 2)
- A **Certified Abutters List(s)** issued within 3 months of this application's filing date which includes all properties affected and includes any contiguous, commonly owned property(s). This can be obtained from the Assessor's Office and includes all abutters and abutters to abutters within 300' of the edge of the land owner's property.
Note: if the property(s) is within 300 ft. of another town an abutters list from that town may be required
- Project Impact Statement** describing the proposed project and analyzing how the project and site layout were designed with consideration for and to be compatible with the review criteria in the Zoning Ordinance.
- Site Plan** showing the full project scope and all elements listed in Item 11 of this application, stamped and signed by all applicable professionals
- Architectural drawings** showing exterior elevation, height in feet and stories, exterior materials for all structures, and corresponding floor plans stamped and signed by all applicable professionals
- Stormwater Report** demonstrating compliance with Massachusetts Stormwater Standards for the project, as applicable based on project type and scope (*contact staff to confirm*)
- Traffic Study**, if necessary based on expected traffic generation (*contact staff to confirm*)

7. PROVIDE 1 PHYSICAL COPY OF THE FOLLOWING ITEMS:

- One stamped (i.e. postage paid) pre-addressed envelope for *each* party on the Abutters List and the applicant (if different from the owner), with the following return address:

Division of Planning and Regulatory Services
455 Main Street (City Hall), Room 404
Worcester, MA 01608

- Filing Fee** of \$ 5,000 is enclosed (*see fee schedule or contact staff to confirm amount*).

8. TAX CERTIFICATION

This certification must be completed by all applicants and owners of the property, certifying payment of all local taxes, fees, assessments, betterments, or any other municipal charges of any kind. Failure to include a completed certification shall result in the application being deemed incomplete.

If a Single Owner or Proprietorship:

- a. _____
Name
- b. _____
Signature certifying payment of all municipal charges
- c. _____
Mailing Address
- d. _____
Email and Phone Number

9. IF A PARTNERSHIP OR MULTIPLE OWNERS:

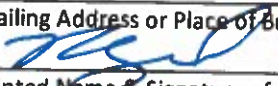
- e. _____
Names
- f. _____
Signatures certifying payment of all municipal charges
- g. _____
Mailing Address
- h. _____
Email and Phone Number

Applicant, if different from owner:

Rossi Development LLC

- i. _____
By: *Anthony Rossi, manager* Anthony Rossi
Printed Name & Signature of Applicant, certifying payment of all municipal charges Manager

If a Corporation or Trust:

- j. Boston Capital Development, LLC
Full Legal Name
- k. Massachusetts 11 Beacon Street, Suite 325, Boston, MA 02108
State of Incorporation Principal Place of Business
- l. 11 Beacon Street, Suite 325, Boston, MA 02108
Mailing Address or Place of Business in Massachusetts
- m.  Richard D. Mazzocchi, Boston Capital Development, LLC
Printed Name & Signature of Owner or Trustee, certifying payment of all municipal charges Authorized Signatory of
- n. _____
Printed Name & Signature of Owner or Trustee, certifying payment of all municipal charges
- o. _____
Printed Name & Signature of Owner or Trustee, certifying payment of all municipal charges
- p. _____
Printed Name & Signature of Owner or Trustee, certifying payment of all municipal charges

10. PROJECT TYPE AND DESCRIPTION

a. Existing Conditions. Describe the current/existing use of the property

The property located at the Proposed Lot 3C is an approximately 5,739 square foot (approximately 0.131 acre) vacant lot, which is currently part of the existing Lot 3A.

b. Proposed Conditions. Check the box for all of the categories that describe the proposed project:

Proposed Project Type			
Residential	<input type="checkbox"/>	New Construction	<input checked="" type="checkbox"/>
Industrial/manufacturing	<input type="checkbox"/>	Rehabilitation/Renovation	<input type="checkbox"/>
Business	<input type="checkbox"/>	Expansion/Addition	<input type="checkbox"/>
Mixed Use	<input checked="" type="checkbox"/>	Change of use	<input type="checkbox"/>
Subdivision	<input type="checkbox"/>	Drive-through	<input type="checkbox"/>
		Gas station	<input type="checkbox"/>
		Lodging House	<input type="checkbox"/>
		Historic Property	<input type="checkbox"/>
		Abuts Historic Property	<input type="checkbox"/>
		Billboard	<input type="checkbox"/>
		Airport Environs Overlay	<input type="checkbox"/>
		≥15% Slope Disturbed	<input type="checkbox"/>

c. Describe the proposed use of the property (attach separate narrative if needed)

The Applicant is proposing to construct a 5-story mixed-use building with approximately 42 residential units (all studios) and two commercial/retail spaces, a retaining wall, landscaping and site improvements related thereto. The property owner will be seeking an approval not required endorsement to create Lot 3C and Lot 3D from the existing Lot 3A.

d. Fill in all information relevant to the proposed project

All Projects	Existing	Change +/-	Total
Overall lot size in square feet	+/- 65,852	- 60,113 SF	+/- 5,739 SF
Number of buildings	0 Buildings	+ 1 Building (Lot 3C)	1 Building (Lot 3C)
Total square footage of building(s)	0 SF	+ Approx. 22,525 SF	Approx. 22,525 SF
Number of stories of building(s)	0 Stories	+ 5 Stories	5 Stories
Number of parking spaces	0 Parking Spaces	N/A	0 Parking Spaces
Number of loading spaces	0 Loading Spaces	N/A	0 Loading Spaces
Changes to on-street parking	N/A	N/A	N/A
Total vehicle daily trips. Please consult staff for specific thresholds requiring review.	See Traffic Memo	See Traffic Memo	See Traffic Memo
Square feet of wetlands	0 SF	N/A	0 SF
Square feet of surface (open) water	0 SF	N/A	0 SF
Square feet of area vegetated/wooded	0 SF	TBD	TBD
Number of trees over 9" in caliper	0 Trees	N/A	0 Trees (over 9") - 2 New Trees (2.5-3")
Cubic yards of fill material to be imported/exported	TBD	TBD	TBD
Square feet of property in floodplain	0 SF	N/A	0 SF
Length of roadway (in feet or miles)	0 ft	N/A	0 ft
Residential	Existing	Change +/-	Total
Number of units	0 Units	+ 42 Units	42 Units
If multi-family, number of bedrooms per unit	0 BR	+ Studios	Studios
Number of accessible units	0 Units	N/A	0 Units
Number of affordable units	0 Units	N/A	0 Units
Business/ Industrial	Existing	Change +/-	Total
Gross square feet of floor area	0 SF	+ 2,248 SF	2,248 SF

11. ZONING

If this project has already been granted Zoning Relief by the Zoning Board of Appeals, please list the relief below:

Type of Relief	Date Approved
Variance from minimum front-yard setback requirement (5 ft required, +/- 0.5 ft proposed, +/- 4.5 ft of relief)	TBD

12. PERMITS REQUIRED

List any Federal, State, or City of Worcester agencies from which permits or other actions have been or will be sought. Please continue list on an attached sheet if needed.

Agency Name	Permit Type	Date Filed	File Number
Worcester Inspectional Services Department	Building Permit	TBD	TBD

13. PLAN REQUIREMENTS

The following information is required of all applications submitted for Site Plan Review. If you are not providing one of these, please check "waiver requested" next to the item.

Feature	Waiver Requested	Location in Set (Sheet/ page #)
a. Site Plan at a minimum 1" = 40'-0" scale, legend, & properly oriented north arrow	<input type="checkbox"/>	All Sheets
b. Locus plan with zoning information shown	<input type="checkbox"/>	C-3
c. Existing and proposed utilities	<input type="checkbox"/>	C-1 and C-4
d. Existing and proposed grading, using differing line types, showing 2' contours	<input type="checkbox"/>	C-1 and C-4
e. Soil types identified on the plan (including test-pit/boring locations for infiltration)	<input checked="" type="checkbox"/>	
f. Architectural elevations and/or renderings (including exterior materials)	<input type="checkbox"/>	Arch. Plans
g. Location of all trees over 9" caliper inches on existing conditions plan	<input type="checkbox"/>	N/A
h. Landscape plan including plantings (location, species, & size), and details for all landscape and hardscape elements	<input type="checkbox"/>	L-1
i. Shade trees to reduce heat island effect. Parking lots >16 spaces: 1 tree per 10 interior surface spaces located to maximize shading effect (Zoning Ordinance, Article IV, Section 7, Table 4.4, Note 6). <i>Landscaping Notes: 1 tree in the front-yard per lot or 1 tree every 50' along street frontage (Subdivision Regulations Section X, J.1.). Trees (3-3.5" caliper) planted every 20-25' where parking areas along a sidewalk or side lot line abutting a residential use, street or public park (Zoning Ordinance, Article IV, Section 5, C. 1.b., 1b.i).</i>	<input type="checkbox"/>	N/A
j. Stormwater mitigation measures for the 2, 10, 25, & 100-year design storm. Provide a stamped Stormwater Checklist & Calculations. All projects shall comply with Massachusetts Stormwater Standards, as applicable to project scale.	<input type="checkbox"/>	Drainage Report
k. For new residential uses in BO, BL, BG-2 and BG-3 Zoning Districts, 10% of lot area has been provided for recreation (Zoning Ordinance Article IV, Section 2, Table 4.2, footnote 3).	<input type="checkbox"/>	N/A

14. REVIEW STANDARDS

The following standards shall be used by the Planning Board in reviewing all applications for site plan review. These standards are intended to provide a frame of reference for the applicant in development of applications. These standards shall not be regarded as inflexible requirements. They are not intended to discourage creativity, invention or innovation. Applicants are encouraged to evaluate the extent to which the site plan, its immediate and general locus and the City more generally can tolerate the development being proposed and adjust their proposals accordingly.

Applicants should additionally provide a narrative "project impact statement" summarizing how the proposed project has been designed with the following criteria in mind by evaluating their proposal on the basis of the following 16 review standards, as outlined in the Zoning Ordinance per Article V, Section 5, B.

Provide the following information about the proposed project in relation to the review standards. If you are not providing one of these features please check "none" next to the item.

1. Adequacy and arrangement of pedestrian traffic access and circulation, walkway structures, control of intersections with vehicular traffic and overall pedestrian convenience.

Feature	None	Page/ sheet #
a. Pedestrian pathways internal to the site, with dimensions of path widths	<input type="checkbox"/>	C-3
b. Pedestrian pathways connecting to sidewalks or nearby amenities	<input type="checkbox"/>	C-3
c. Doors/egress to all existing and proposed buildings	<input type="checkbox"/>	C-3 and Arch. Plans
d. Pedestrian paving and surface treatment details	<input type="checkbox"/>	C-3
e. Safe, ADA accessible pedestrian crossings at driveways and intersections	<input type="checkbox"/>	C-1 and C-3

2. Adequacy and arrangement of vehicular traffic access and circulation including intersections, road widths, pavement surfaces, dividers and traffic controls.

Feature	None	Page/ sheet #
a. Driveway layout & materials	<input checked="" type="checkbox"/>	
b. Dimensions of all drives and curb cut widths, minimizing the number and width of curb-cuts (see Note 5 to Table 4.4)	<input checked="" type="checkbox"/>	
c. Access control and directional signage (e.g. gates, pavement markings, etc.)	<input checked="" type="checkbox"/>	
d. Pavement and curb details, including level sidewalks at driveways	<input type="checkbox"/>	C-3
e. Permeable or porous paving, and/ or cool pavements/ treatments	<input checked="" type="checkbox"/>	

3. Location, arrangement, appearance and sufficiency of off-street parking and loading.

Feature	None	Page/ sheet #
a. Number of parking spaces provided (9 x 18)	<input checked="" type="checkbox"/>	
b. Number of compact parking spaces (8 x16)	<input checked="" type="checkbox"/>	
c. ADA parking spaces	<input checked="" type="checkbox"/>	
d. Parking aisle width (24 feet for 90° parking; see policy for angled spaces)	<input checked="" type="checkbox"/>	
e. Parking is outside front & exterior side yard/setback (except residential drives)	<input checked="" type="checkbox"/>	
f. Loading spaces or docks (see Table 4.5 and related notes)	<input checked="" type="checkbox"/>	
g. Screen planting between parking and edge of property or pedestrian paths	<input checked="" type="checkbox"/>	
h. Number of electric vehicle charging stations or "ready" (conduit run) spaces	<input checked="" type="checkbox"/>	
i. Bicycle parking (is it covered, or provided inside the building? Circle: YES NO)	<input checked="" type="checkbox"/>	

4. Location, arrangement, size, design and general site compatibility of buildings, lighting and signs.

Feature	None	Page/ sheet #
a. Building entrance fronting on the sidewalk	<input type="checkbox"/>	C-3 and Arch. Plans
b. Front façade with features to add visual interest and activate street (e.g., window placement, variation of materials, reduction in massing, etc.)	<input type="checkbox"/>	Arch. Plans
c. Green roof, blue roof, rooftop solar, or use of high-albedo roof treatments	<input checked="" type="checkbox"/>	
d. Light levels appropriate for safety (1 foot candle) where pedestrians and vehicles will meet	<input type="checkbox"/>	TBD
e. Parking and circulation directional signage	<input checked="" type="checkbox"/>	
f. Signage facing the street	<input type="checkbox"/>	TBD

5. Adequacy of stormwater and drainage facilities.

Feature	None	Page/ sheet #
a. Flood Zones, wetlands, watercourses, and water quality and wellhead protection areas	<input checked="" type="checkbox"/>	
b. Bioswale or other open stormwater infiltration area planted with native vegetation (rain garden, etc.)	<input checked="" type="checkbox"/>	
c. Infiltration of clean runoff to maintain groundwater supply	<input type="checkbox"/>	Drainage Report
d. Overflow or other connection to City stormwater infrastructure***	<input type="checkbox"/>	C-4

***Contact DWP&P to determine any applicable sewer connection or use change fees.

6. Adequacy of water supply and sewerage disposal facilities.

Feature	None	Page/ sheet #
a. Connections to or extensions of city sanitary sewer and water utilities. Contact DWP&P to determine any applicable sewer connection or use change fees.	<input type="checkbox"/>	C-4
b. Connections to or extensions of city storm drainage infrastructure	<input type="checkbox"/>	C-4
c. Footing or foundation drainage for a proposed structure or wall	<input type="checkbox"/>	TBD

7. Adequacy, type and arrangement of trees, shrubs and other landscaping elements in accordance with the Landscaping Design Standards set forth in Article V, Section-5(C).

Feature	None	Page/ sheet #
a. Walls, including height (show top & bottom elevations at highest and all intersecting points, minimize height whenever possible), materials, and related drainage.	<input type="checkbox"/>	C-4
b. Engineered slopes (rip-rap is not recommended)	<input checked="" type="checkbox"/>	
c. Planted buffers between parking facilities and adjacent properties or roads	<input checked="" type="checkbox"/>	
d. Proposed plantings and areas to be seeded (number, species or mix, size)	<input type="checkbox"/>	L-1
e. Fencing, including information on material, height, and style (including gates)	<input checked="" type="checkbox"/>	
f. Planted buffers along rear and side yard setbacks	<input type="checkbox"/>	L-1

8. In the case of an apartment complex or other multiple dwelling, the adequacy of useable common property or open space. Note: for residential uses in Business Districts see Article IV, Section 2, Table 4.2, footnote 3.

Feature	None	Page/ sheet #
a. Outdoor seating (i.e. benches, seat walls, picnic tables, etc.)	<input checked="" type="checkbox"/>	
b. Recreation or play area (Is it designed for children/ families? Circle: YES NO)	<input checked="" type="checkbox"/>	
c. Raised beds for a community garden or other urban agriculture provisions	<input checked="" type="checkbox"/>	
d. Paved pedestrian plaza area (includes patios) or deck	<input type="checkbox"/>	C-3
e. Interior common space and amenities or balconies	<input type="checkbox"/>	Arch. Plans

9. Protection of adjacent or neighboring properties against noise, glare, unsightliness or other objectionable features.

Feature	None/	Page/ sheet #
a. Plan locating all existing (to remain) & proposed light fixtures	<input type="checkbox"/>	TBD
b. Details of all proposed light fixtures: showing max temperature of 4,000K, dark-sky compliant, and with shielding to prevent light spillover	<input type="checkbox"/>	TBD
c. Photometric plan for parking lots with ≥12 new spaces	<input checked="" type="checkbox"/>	
d. Opaque fencing or evergreen planting to screen trash or utility areas (including siting and screening of roof-top equipment, as applicable)	<input type="checkbox"/>	L-1
e. Sound attenuation at loading, utility, and other noise generating areas with particular attention to sensitive neighbors	<input checked="" type="checkbox"/>	
f. Limit of clearing, with mature vegetation protected where possible	<input checked="" type="checkbox"/>	N/A

10. Adequacy of fire lanes and other emergency zones and the provisions of fire hydrants.

Feature	None	Page/ sheet #
a. Diagram of fire truck access path (applicant should coordinate turning radius and access requirements with the Fire Department)	<input checked="" type="checkbox"/>	
b. Clearly marked fire or emergency loading areas	<input checked="" type="checkbox"/>	
c. Fire hydrants and/or FDC connections	<input type="checkbox"/>	C-1

11. Special attention to the adequacy of structures, roadways and landscaping in areas with susceptibility to ponding, flooding and/or erosion.

Feature	None	Page/ sheet #
a. All buildings and utilities are located at or above the 500-year flood elevation	<input checked="" type="checkbox"/>	
b. Drainage infrastructure is designed to reduce ponding and slow runoff	<input type="checkbox"/>	Drainage Report

12. Adequacy of erosion and sedimentation control measures to be utilized during and after construction.

Feature	None	Page/ sheet #
a. Erosion control plan narrative sequence (including perimeter controls and temporary stormwater management) for construction activities	<input type="checkbox"/>	C-2
b. Plans for securing of any stockpiles on site during construction	<input checked="" type="checkbox"/>	
c. Temporary and permanent slope stabilization/designs for slopes greater than 3H:1V; (note: loam and seed is not sufficient)	<input checked="" type="checkbox"/>	
d. Slopes ≥2.5H:1V are engineered (note: loam and seed is not sufficient)	<input checked="" type="checkbox"/>	
e. Temporary sediment basins and other means of stormwater velocity attenuation or conveyance proposed during construction	<input checked="" type="checkbox"/>	

13. Conformance and compatibility of the site plan design with structures listed in the most recent State Register of Historic Places.

Feature	None	Page/ sheet #
a. Protection of existing historic architectural or site features	<input checked="" type="checkbox"/>	
b. Historically-sensitive façade, window, and roof treatments	<input checked="" type="checkbox"/>	

14. Adequacy and impact on the regional transportation system.

Feature	None	Page/ sheet #
a. Bus service within ¼ mile (indicate number of stops and route numbers)	<input checked="" type="checkbox"/>	
b. Improvements to neighborhood walk/bike-ability or public transportation	<input type="checkbox"/>	C-3

15. Adequacy of plans and protective measures to ensure minimal risk of contamination to surface or ground water.

Feature	None	Page/ sheet #
a. Snow storage locations (outside of basins and required parking/landscape buffer)	<input checked="" type="checkbox"/>	
b. Water quality structures to remove total suspended solids (TSS) from runoff	<input type="checkbox"/>	C-4
c. Water quality structures to remove pollutants from runoff (i.e. oil/ water separators, etc.)	<input checked="" type="checkbox"/>	
d. Plan for mitigation of any contaminated soils (include RTN, RAM Plan, AUL)	<input checked="" type="checkbox"/>	N/A
e. Locations of material to cut or filled (including the location of the source material if fill)	<input type="checkbox"/>	TBD
f. Dewatering plans	<input checked="" type="checkbox"/>	

16. Conformance of the site design with the purposes and intent of the Worcester Zoning Ordinance.

Feature	None	Page/ sheet #
a. Minimum yard setbacks (for front, side, and rear)	<input type="checkbox"/>	C-3
b. Property and right-of-way boundary lines (include the status of ways)	<input type="checkbox"/>	C-1 and C-3
c. Easements for any utilities, public access, or adjacent properties	<input checked="" type="checkbox"/>	
d. Regularity factor for all lots	<input type="checkbox"/>	C-3
e. % paving within the front-yard for residential uses	<input checked="" type="checkbox"/>	N/A
f. Height of all structures in feet and stories	<input type="checkbox"/>	Arch. Plans

**Project Impact Statement and Statement in Support for Rossi Development LLC's
Application to City of Worcester Planning Board for Definitive Site Plan Review
for Mixed-Use Development
at 120 Washington Street (Proposed Lot 3C), Worcester, Massachusetts**

I. Background, Preliminary Site Plan Approval and Project Scope.

The property known and numbered as 120 Washington Street, Worcester, Massachusetts (the "Washington Street Property") was previously occupied by a Table Talk Pies manufacturing facility with related loading and parking areas and consists of the following six (6) parcels which are more particularly shown on the plan entitled, "Approval Not Required Plan of Land Boston Capital Development, LLC", prepared by Control Point Associates, Inc., dated April 4, 2022 and recorded with the Worcester District Registry of Deeds in Plan Book 963, Plan 91 (the "Recorded ANR Plan"):

- (i) "Proposed Lot #1" consists of approximately 34,017 square feet of land on the corner of Madison Street and Washington Street ("Lot 1")¹ and is occupied by a 7-story mixed-use building known as District 120 with 83 affordable residential units, a 1,750 square foot ground floor retail/commercial unit and 17 surface parking spaces;
- (ii) "Proposed Lot #2" consists of approximately 16,236 square feet of land on the corner of Green Street and Spruce Street ("Lot 2")² and is occupied by a 3-story mixed-use building with approximately 56 residential units and approximately 6,500 square feet of ground floor retail/commercial space;
- (iii) "Proposed Lot #3A" consists of approximately 65,852 square feet of vacant land with frontage along Washington Street, Spruce Street and Ash Street and two extended areas along Green Street which are on either side of the property known as 139 Green Street ("Lot 3A")³;
- (iv) "Proposed Lot #3B" consists of approximately 32,072 square feet of vacant land with frontage along Madison Street and Spruce Street ("Lot 3B")⁴;
- (v) "Proposed Lot 4" consists of approximately 4,119 square feet of vacant land along Madison Street ("Lot 4")⁵; and
- (vi) "Proposed Lot #5" consists of approximately 17,733 square feet which has been improved to be a right of way known as Spruce Street which connects Washington Street to Green Street (the "Spruce Street Parcel")⁶.

¹ Lot 1 is identified as Parcel 05-0005-00001.

² Lot 2 is identified as Parcel 05-0005-0002A.

³ Lot 3A is identified as Parcel 05-0005-0003A.

⁴ Lot 3B is identified as Parcel 05-005-0003B.

⁵ Lot 4 is identified as Parcel 05-005-0004A.

⁶ The Spruce Street Parcel is identified as Parcel 05-005-0005A.

The Washington Street Property is located exclusively within the Business, General (BG-6.0) zoning district, the Commercial Corridors Overlay District – Downtown (CCOD-D) and the Downtown/Blackstone Canal Sign Overlay District (DSOD).

Boston Capital Development, LLC (the “Property Owner”) currently owns Lot 3A, Lot 3B, Lot 4 and the Spruce Street Parcel. The Property Owner intends to submit a new approval not required (“ANR”) plan to the Planning Board (the “Board”) for endorsement to further separate Lot 3A into the following three parcels: (i) approximately 54,845 square feet of vacant land with frontage along Washington Street, Spruce Street and Ash Street (the “New Lot 3A”); (ii) approximately 5,739 square feet of vacant land on the corner of Spruce Street and Green Street and located to the south of 139 Green Street (“Proposed Lot 3C”); and (iii) approximately 5,269 square feet of vacant land on the corner of Ash Street and Green Street and located to the north of 139 Green Street (“Proposed Lot 3D”). A copy of the ANR plan showing the New Lot 3A, Proposed Lot 3C and Proposed Lot 3D is enclosed herewith.

On February 1, 2023, Quarterra Multifamily Communities, LLC (the “Initial Developer”) obtained preliminary site plan approval (the “Preliminary Site Plan Approval”) from the Board in connection with: (i) a proposed 8-story 395,000 square foot mixed-use building with approximately 375 dwelling units and 22,000 square feet of ground floor commercial space on Lot 3A and the property known as 139 Green Street; and (ii) a 7-story detached parking garage with approximately 560 parking spaces on Lot 3B (collectively, the “Approved Project”).

Ultimately, the Initial Developer was unable to finance the construction of the Approved Project due increases in construction costs and high interest rates. The Initial Developer subsequently assigned its rights in the Approved Project to the Property Owner. On February 28, 2024, upon request of the Property Owner, the Board granted an extension of time (“EOT”) for the Preliminary Site Plan Approval for the Approved Project. Since the grant of the extension of time, the Property Owner has been evaluating modifications to the Approved Project to ensure the developability of Lot 3A and Lot 3B and meet the recommendations of the Board provided in Preliminary Site Plan Approval and EOT decisions.

The Approved Project is now being modified from one mixed-use building and a parking structure into 4 separate, new developments (the “New Projects”) which will be developed as follows:

- (i) SMC Residences at Table Talk Square Limited Partnership seeks to develop a 7-story, approximately 282,359 square foot, multifamily building on the New Lot 3A with approximately 185 dwelling units (consisting of 29 studios, 86 1-bedroom units and 70 2-bedroom units) within the upper 5 floors, 2 lower levels of podium parking with a total of 230 parking spaces (113 parking spaces on the first floor and 117 parking spaces on the second floor), amenity space, 2 outdoor courtyards, new trees and landscaping, a new transformer area and site improvements related thereto;

- (ii) Rossi Development LLC (“Rossi”) seeks to develop a 7-story mixed-use building (the “New Building”) on Lot 3B with approximately 90 residential units, two commercial/retail units for a total of 1,650 square feet of space, pervious paver outdoor patio area, new trees, landscaping, a retaining wall, transformer, 78 podium and surface parking spaces and site improvements related;
- (iii) Rossi seeks to develop a 5-story mixed-use building (the “New Building”) on Lot 3C (which will be created through the new ANR plan) with approximately 42 residential units (all studios), two commercial/retail spaces, a retaining wall, a transformer, new sidewalk/walkway areas and landscaping along Green Street and Spruce Street (the “Lot 3C Project”); and
- (iv) Rossi seeks to develop a 5-story mixed-use building on Lot 3D (which will also be created through the new ANR plan) with approximately 40 residential units (all studios) and two commercial/retail spaces along Green Street and Ash Street.

The New Projects are designed to satisfy the required modifications set forth in the Preliminary Site Plan Approval and EOT decisions, to the extent applicable given the broader changes to the Approved Project. In particular, the New Projects are intended to promote safe and efficient pedestrian and vehicular circulation and meet the purposes and intent of Commercial Corridors Overlay District (“CCOD”) as set forth in Article IX of the Zoning Ordinance.

Rossi respectfully requests definitive site plan approval in connection with the development of the Lot 3C Project.

For the avoidance of any doubt, the request for definitive site plan approval as set forth herein applies only to the Lot 3C Project. Separate definitive site plan approval applications are being filed simultaneously with the Board for the remaining New Projects.

II. Requirement for Definitive Site Plan Review and Other Approvals

The development of 5 or more dwelling units requires site plan review approval by the Board pursuant to Table 5.1 of Article V of the City of Worcester Zoning Ordinance (the “Zoning Ordinance”). Given that the Lot 3C Project will result in the construction of 42 dwelling units, the Lot 3C Project requires site plan review pursuant to Article V of the Zoning Ordinance.

The Property Owner is concurrently filing an application for an ANR endorsement to establish the New Lot 3A, Lot 3C and Lot 3D.

III. Compliance with Site Plan Review Approval Criteria.

The Project satisfies the site plan review criteria as set forth in Article V, Section 5.B of the Zoning Ordinance for the reasons stated herein:

1. Adequacy and arrangement of vehicular traffic access and circulation including intersections, road widths, pavement surfaces, dividers and traffic controls; adequacy and arrangement of pedestrian traffic access and circulation, walkway structures, control of intersections with vehicular traffic and overall pedestrian convenience; location, arrangement, appearance and sufficiency of off-street parking and loading.

The Lot 3C Project will provide a safe, adequate and efficient layout and design for pedestrian traffic along Spruce Street and Green Street. The two commercial/retail units will have individual alcove entries prominently located on Green Street adjacent to the sidewalk. The southerly commercial/retail unit will also have a dedicated door along Spruce Street. A separate door on Spruce Street will provide access to an internal corridor which will provide access to the southerly and northerly commercial/retail units and the remainder of the New Building. The main entrance to the New Building for residents will be located on Green Street. There will be additional residential entrances to the New Building on Spruce Street, offering residents multiple access points to their units. The Lot 3C Project will result in the widening of the concrete sidewalk area adjacent to the sidewalk to provide more room for pedestrians at the corner of Green Street and Spruce Street. The Lot 3C Project, in its entirety, will be constructed to the north of the newly constructed Spruce Street which has new sidewalks, crosswalks, and curb ramps with detectable warning surfaces. Green Street also has crosswalks providing pedestrians safe and efficient access to the more easterly portion of the Canal District.

As provided above, the Lot 3C Project will consist of approximately 42 residential units. While Article IV, Table 4.4 of the Zoning Ordinance provides certain minimum parking requirements (e.g., 2 spaces per dwelling unit) and Article IV, Table 4.5 of the Zoning Ordinance provides certain minimum loading requirements for multifamily high-rise units, Lot 3C is located within the BG-6.0 and the CCOD-D and therefore does not have minimum off-street parking or loading requirements pursuant to Article IX, Section 7, Table 9.1, Note 1 of the Zoning Ordinance. The Lot 3C Project is in close proximity to ample public transit options and leased parking options off-site. The Lot 3C Project, like the other New Projects, is a transit-oriented development which is located in close proximity to Union Station and the Worcester Regional Transit Authority bus terminal. There are two main bus lines (Routes 4 and 11) along Green Street with stops in close proximity to the Proposed Lot 3C.

According to a Traffic Memorandum prepared by The Engineering Corp. (“TEC”) dated December 12, 2024 (the “Traffic Memorandum”), the New Projects (as defined above) will require a total of 111 parking spaces. As provided in the Traffic Memorandum, the proposed 308 parking spaces in the New Projects, in addition to the existing parking spaces on Lot 1, will be sufficient to meet the typical demands of the New Projects and the existing developments on the Washington Street Property. Moreover, there are additional on-street parking stalls and public and private parking facilities throughout the neighborhood to provide additional parking for peak parking demands. Based on the foregoing, the proposed parking space supply is expected to

reasonably support the New Projects based on ITE Parking Generation 11th Edition demand rates.

According to the Traffic Memorandum, the New Projects will result in an increase of 192 weekday morning peak hour trips and 254 weekday evening peak hour trips. TEC concludes that these new vehicle trips can be efficiently and safely accommodated on the surrounding streets and that the multi-modal transportation system available in the area can accommodate the New Projects' needs.

2. Location, arrangement, size, design and general site compatibility of buildings, lighting and signs; protection of adjacent or neighboring properties against noise, glare, unsightliness or other objectionable features.

The Lot 3C Project is functionally and aesthetically compatible with the surrounding residential and commercial properties in the neighborhood, which include a mix of commercial, multifamily and mixed-use buildings. The proposed mixed-use building is permitted by right at the Proposed Lot 3C and therefore is compatible and in harmony with the neighborhood's character. The New Building will dramatically improve the streetscape within the Canal District by converting a vacant, overgrown lot into a modern, new mixed-use building that will activate the corner of Green Street and Spruce Street.

The kind, size, height and nature of the New Building's 5-story height will strike an appropriate balance with other multifamily and mixed-use buildings in the neighborhood (e.g., 153 Green Street is 3-stories; Worcester Public Market and Kelley Square Lofts at 150 Green Street is 4-stories; District 120 at 120 Washington Street is 7-stories; Rossi's proposed mixed-use building on Lot 3B is 7-stories and Rossi's proposed mixed-use building on the Proposed Lot 3D is 5-stories). The New Building will have architecturally appealing features, including juliet balconies, canopies visible from Green Street and Spruce Street to highlight the retail spaces and variations in exterior wall materials (e.g. brick, hardi panel and wood accents), tones and textures, all of which will be visible from abutting streets. The New Building will comply with all dimensional requirements of the Zoning Ordinance except for the minimum front-yard setback, for which the Rossi has requested a variance from the City of Worcester Zoning Board of Appeals.

The Lot 3C Project will neither create a nuisance, hazard, congestion or concerns pertaining to health, safety or general welfare, and nor will it result in substantial harm to the neighborhood or derogation from the intent of the Zoning Ordinance because the proposed uses are consistent with those in the neighborhood.

The proposed outdoor lighting, which includes light poles and wall-mounted lighting, will be adequate for safe and secure access to and from the New Building. The proposed lighting will be shielded fixtures and appropriately arranged with directional shields so as to minimize light from shining and/or spilling onto abutting properties or streets and will not have a deleterious

effect on neighboring properties.

Trash and recycling receptables will be located within the New Building and will therefore be away from public view.

3. Adequacy of stormwater and drainage facilities; adequacy of water supply and sewerage disposal facilities; adequacy of plans and protective measures to ensure minimal risk of contamination to surface or ground water.

New water and sewer connections, and electric service facilities and infrastructure will be required for the New Building. Rossi has confirmed with the City of Worcester Department of Public Works & Parks and the applicable utility companies that sufficient utility lines and infrastructure are available to service the Lot 3C Project in Spruce Street.

The development does not anticipate any adverse effect on drainage patterns. The best management practices (“BMPs”) for stormwater are incorporated in the design of the Lot 3C Project, and will be adequate to manage stormwater runoff generated in satisfaction of the requirements of the Zoning Ordinance, the Worcester Department of Public Works & Parks and Massachusetts Stormwater standards. Said standards include provisions relative to the treatment of stormwater runoff and equivalent peak rate runoff. Stormwater from the proposed roof area will be captured via roof drain and discharged directly to the closed drainage system in Green Street. The inclusion of landscaped areas will reduce the total impervious area from the pre-development condition and reduce peak rates. The Lot 3C Project will also improve the quality of stormwater runoff being directed to the closed drainage system.

4. Adequacy, type and arrangement of trees, shrubs and other landscaping elements in accordance with the Landscaping Design Standards set forth in Article V, Section 5.C.

The Lot 3C Project will include new landscaping, featuring 2 new trees and 4 shrubs, along the westerly boundary. All of the trees will have a minimum caliper of 2.5-3”. All new trees will be Asian Long-Horned Beetle and Emerald Ash Borer resistant.

5. Adequacy of useable common property or open space.

The Lot 3C Project includes two commercial/retail spaces on the ground floor that will service both residents and the public. The Lot 3C Project is ideally located near Polar Park and all of the commercial establishments (e.g., Worcester Public Market and Crompton Place) within the Canal District, offering residents easy access to explore and engage with the surrounding community.

6. Adequacy of fire lanes and other emergency zones and the provisions of fire hydrants.

There are existing fire hydrants along Spruce Street and Green Street.

7. Special attention to the adequacy of structures, roadways and landscaping in areas with susceptibility to ponding, flooding and/or erosion; adequacy of erosion and sedimentation control measures to be utilized during and after construction.

The Lot 3C Project's site design and proposed drainage will reduce any susceptibility of ponding, flooding and erosion. The Proposed Lot 3C is located outside of the Floodplain District and ecologically sensitive areas, and there are no wetland resource areas on or adjacent thereto. During construction, appropriate measures will be taken to control erosion, sedimentation and pollution. Straw wattles and silt fence will be placed downhill at the limit of work and will remain until the site is stabilized.

8. Conformance of site design with the purposes and intent of the Worcester Zoning Ordinance.

The Lot 3C Project is in conformance with the purposes and intent of the Zoning Ordinance, as it will create housing of the type, size and cost suitable for meeting the current and future needs of the City. Moreover, the Lot 3C Project will encourage the most appropriate use of the land in a manner that protects architectural, scenic and aesthetic qualities of the community and protects against the uses of land which are incompatible with nearby uses, undue intensity of noise and danger and congestion in travel and transportation.

The Lot 3C Project's thoughtful site planning and building design will satisfy the purposes and intent of the CCOD by creating a compact development of compatible land uses at urban densities, reusing and redeveloping an existing undersized lot, contributing to a high-quality, pedestrian-scale environment that provides an aesthetically pleasing pedestrian environment that is accessible, compact and inviting, and avoiding over-dedication of land to surface parking by preventing excessive accessory off-street parking. While the Lot 3C Project requires a variance for the minimum front-yard setback, the reduced setback of the New Building aligns with other residential buildings in the Canal District with ground floor residences that have limited or no front yard setback (e.g., Rossi's mixed-use development on Lot 2 and Walker Lofts). This is in keeping with CCOD's goal of offering flexibility with regard to dimensional requirements in a manner that is consistent with the purposes and intent of Article IX.

The Lot 3C Project will comply with the CCOD design guidelines set forth in Article IX, Section 6.B by ensuring that the New Building sits adjacent to the abutting streets and sidewalks, has primary entrances along Spruce Street and Green Street, presents an active façade with windows, changes in materials, modulation of the façade and the use of canopies along the sides of the New Building.

9. Conformance and compatibility of the site plan design with structures listed in the most recent State Register of Historic Places.

The Proposed Lot 3C is not listed on the National Register of Historic Places or the Massachusetts Cultural Resource Information System (MACRIS) database. The Lot 3C Project will be compatible with historic and non-historic structures in the neighborhood.

10. Adequacy and impact on the regional transportation system.

The Lot 3C Project will not materially impact the regional transportation system, as residents of the New Building will benefit from ample public transit options, including Union Station and existing bus stops conveniently located near the Proposed Lot 3C along Green Street.



PROPOSED MIXED-USE DEVELOPMENT

Lot 3C Green Street
Worcester, Massachusetts 01610

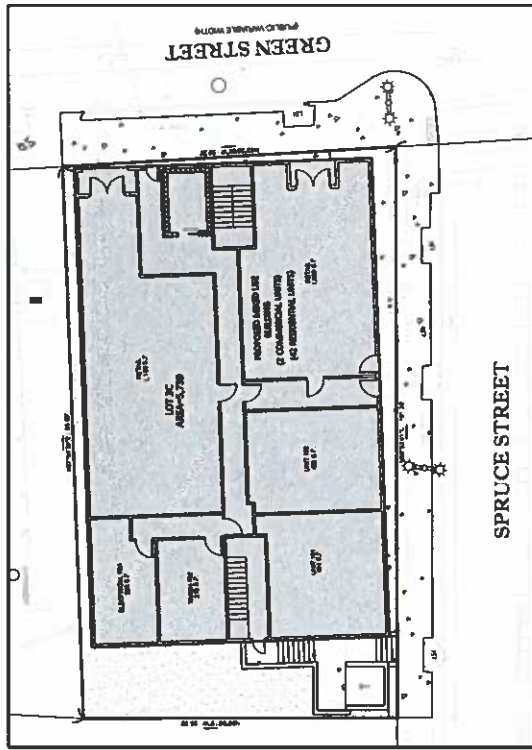


LOCUS MAP
SCALE: 1:35,000

OWNER/APPLICANT:
ROSSI DEVELOPMENT
345 BOYLSTON STREET SUITE 300
NEWTON, MA 02459

CIVIL ENGINEER:
ENGINEERING ALLIANCE, INC.
194 CENTRAL STREET
SALGUS, MA 01906
(781) 231-1349

ARCHITECT:
PAUL R. LESSARD
13 STATION ROAD
SALEM, MA 01970
(978) 210-1960




- SHEET NUMBER AND TITLE**
- C-0 COVER SHEET
 - C-1 EXISTING CONDITIONS PLAN
 - C-2 EROSION CONTROL PLAN
 - C-3 SITE LAYOUT PLAN
 - C-4 GROUND DRAINAGE & UTILITY PLAN
 - L-1 LANDSCAPE PLAN
 - D-1 CONSTRUCTION DETAILS I

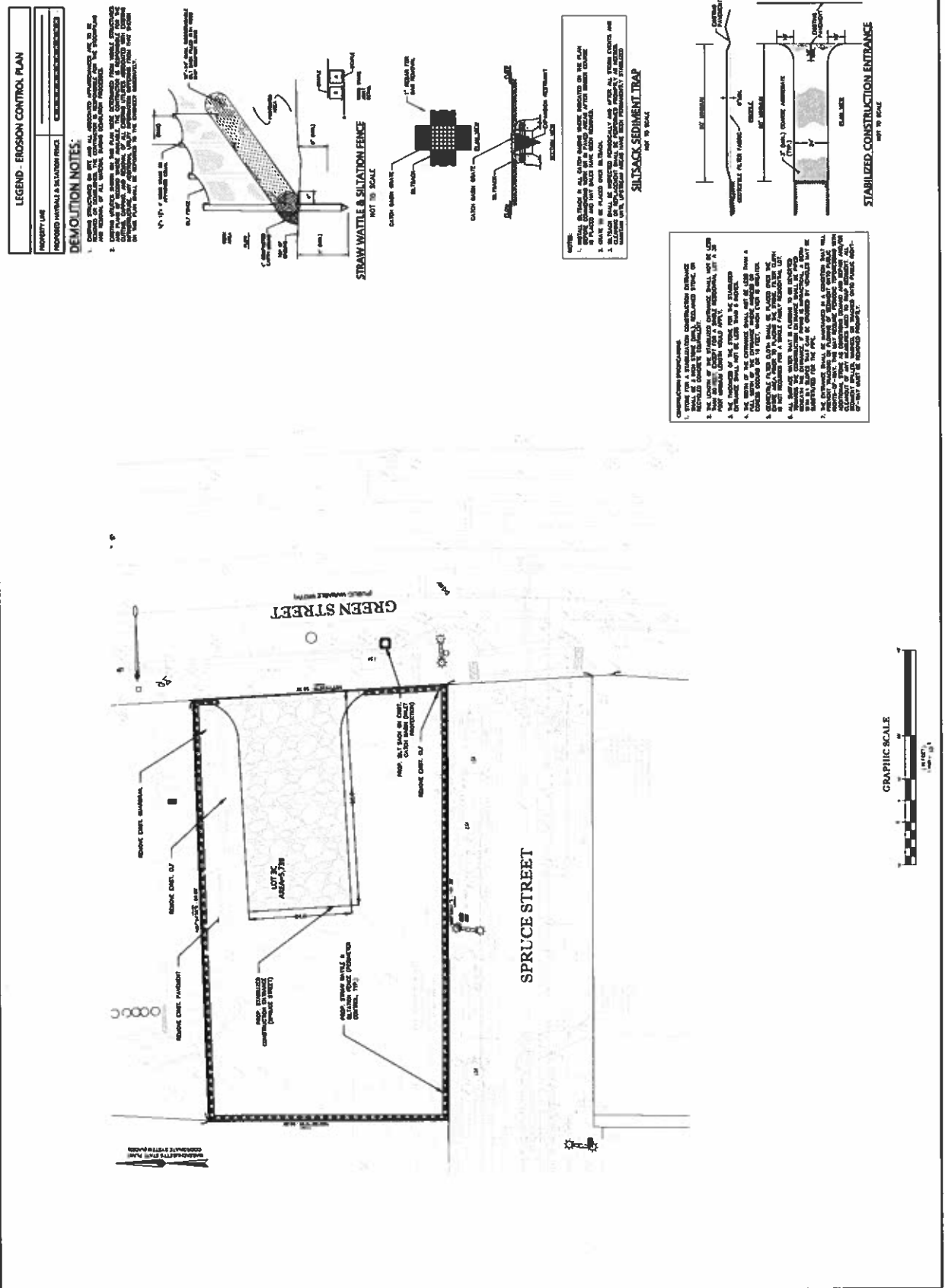
PREPARED FOR:
Rossi Development
345 Boylston Street Suite 300
Newton, MA 02459

PREPARED BY:



Engineering Alliance, Inc.
Civil Engineering & Land Planning Consultants
194 Central Street
Salgus, MA 01906
Tel: (781) 231-1349
Fax: (781) 417-4820

 PROJECT: Proposed Site Plan Lot 3C Green Street Worcester, Massachusetts	DATE: 	SCALE AS NOTED: 	PROJECT #: 2014-03	OWNER: Rossi Development, Inc. 345 Boylston Street, Suite 300 Newton, MA 02459 Tel: (781) 417-4820	DATE:	PROJECT #: 2014-03	SCALE AS NOTED: 	OWNER: Rossi Development, Inc. 345 Boylston Street, Suite 300 Newton, MA 02459 Tel: (781) 417-4820	DATE:	PROJECT #: 2014-03
	DATE:	SCALE AS NOTED: 	PROJECT #: 2014-03	OWNER: Rossi Development, Inc. 345 Boylston Street, Suite 300 Newton, MA 02459 Tel: (781) 417-4820	DATE:	PROJECT #: 2014-03	SCALE AS NOTED: 	OWNER: Rossi Development, Inc. 345 Boylston Street, Suite 300 Newton, MA 02459 Tel: (781) 417-4820	DATE:	PROJECT #: 2014-03

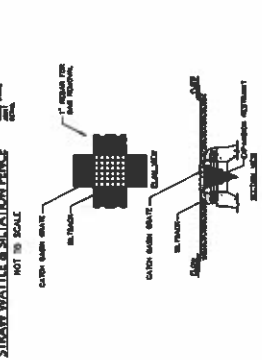
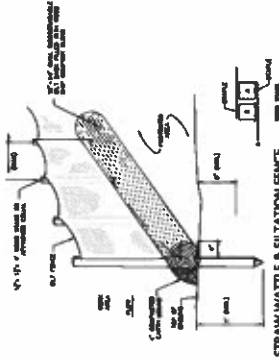


LEGEND - EROSION CONTROL PLAN

PROPOSED DRIVEWAY	XXXXXXXXXXXXXXXXXXXX
PROPOSED WALKWAY & SILLIATION FENCE	XXXXXXXXXXXXXXXXXXXX

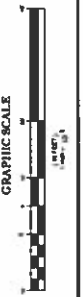
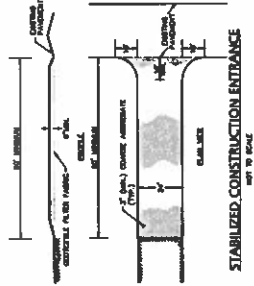
DEMOLITION NOTES:

1. ALL EXISTING CONSTRUCTION SHALL BE DEMOLISHED AND THE AREA REVEALED TO THE ORIGINAL GRADE.
2. EXISTING CONSTRUCTION SHALL BE DEMOLISHED AND THE AREA REVEALED TO THE ORIGINAL GRADE.
3. EXISTING CONSTRUCTION SHALL BE DEMOLISHED AND THE AREA REVEALED TO THE ORIGINAL GRADE.



- NOTES:**
1. ALL DEMOLITION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.
 2. ALL DEMOLITION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.
 3. ALL DEMOLITION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.

- CONSTRUCTION PRECAUTIONS:**
1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.
 2. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.
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 8. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE MASS DEPARTMENT OF ENVIRONMENTAL AFFAIRS (DEA) REGULATIONS.





PROJECT: Proposed Site Plan
 Lot 3C Green Street
 Worcester, Massachusetts

DATE: 07/20/2017
 DRAWN BY: J. B. BROWN

SCALE: AS NOTED

CHECKED BY: MICHAEL A. BROWN, P.E.
 MICHAEL A. BROWN, P.E.
 License No. 13112
 Exp. 07/20/2020
 Engineering Alliance, Inc.
 1500 South Main Street
 Worcester, MA 01602
 Tel: (508) 853-1300
 Fax: (508) 853-1300

DATE	DESCRIPTION OF REVISION

LEGEND - SITE LAYOUT PLAN

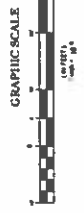
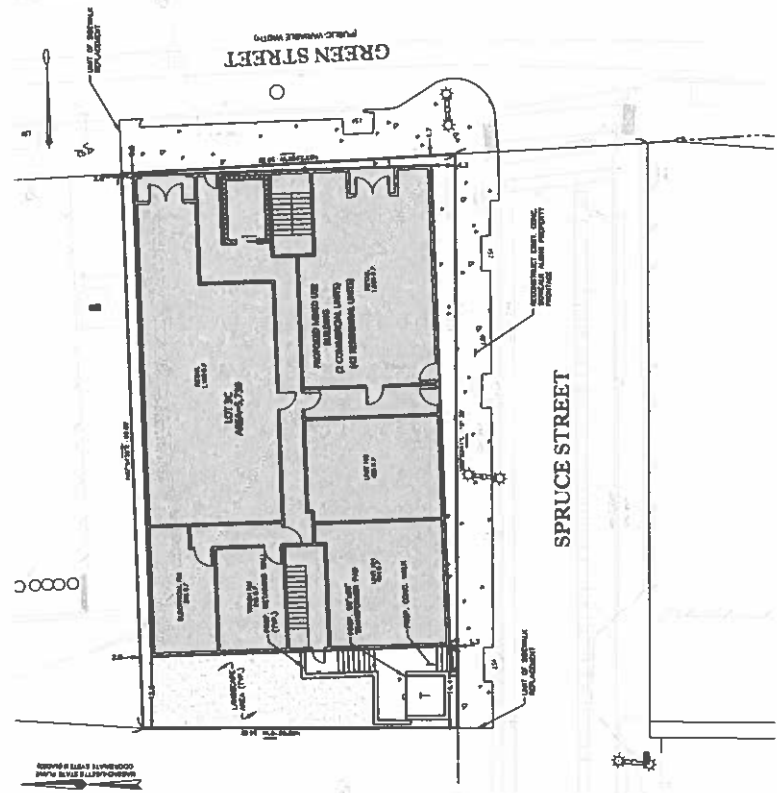
PROPERTY LINE	
EXISTING BUILDING	
EXISTING EDGE OF PAVEMENT	
EXISTING CURB	
PROPOSED BUILDING	
PROPOSED CONC. PAVEMENT	
PROPOSED LANDSCAPE AREA	
PROPOSED STRIPING	
COMPACT SPACES (R111F)	

LAND USAGE TABLE
 ALL AREAS SHOWN UNLESS OTHERWISE NOTED. CALCULATIONS BASED ON CURRENT DISTRICT.

ITEM	REQUIRED (SQUARE FEET)	REQUIRED (COUNTRIES)	PROVIDED
MINIMUM LOT AREA	5,000 S.F.	-	5,796 S.F.
MINIMUM LOT FRONTAGE	40 FEET OR UP TO 200 FT	-	50.58 FT
MIN. REGULABILITY FACTOR	0.40	-	0.82
MAX FLOOR AREA RATIO	6.1	-	3.0
MINIMUM FRONT YARD SETBACK (GREEN ST.)	-	-	0.5 FT
MINIMUM SIDE YARD SETBACK	-	-	1.3 FT
MINIMUM REAR YARD SETBACK	-	-	12.8 FT
MAXIMUM HEIGHT	-	-	50 FT

PARKING CALCULATIONS

COMPONENT	REQUIRED	PROVIDED
42 RESIDENTIAL UNITS @ 1.5 SPACES PER RESIDENTIAL UNIT	63 SPACES	6 SPACES
2 RETAIL UNITS @ 10 SPACES PER RETAIL UNIT	20 SPACES	6 SPACES
TOTAL	83 SPACES	12 SPACES





PROJECT: Proposed Site Plan
 Lot 3C Green Street
 Worcester, Massachusetts

PROJECT # 34252

SCALE AS NOTED

DESIGN BY: DAVID S. ROSSI, P.E.
 CHECKED BY: MICHAEL A. SHAW, P.E.
 DATE: 01/11/2011

DESIGNED BY: MICHAEL A. SHAW, P.E.
 CHECKED BY: DAVID S. ROSSI, P.E.
 DATE: 01/11/2011



DATE	DESCRIPTION OF REVISION

LEGEND - GRADING, DRAINAGE & UTILITY PLAN

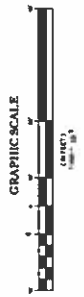
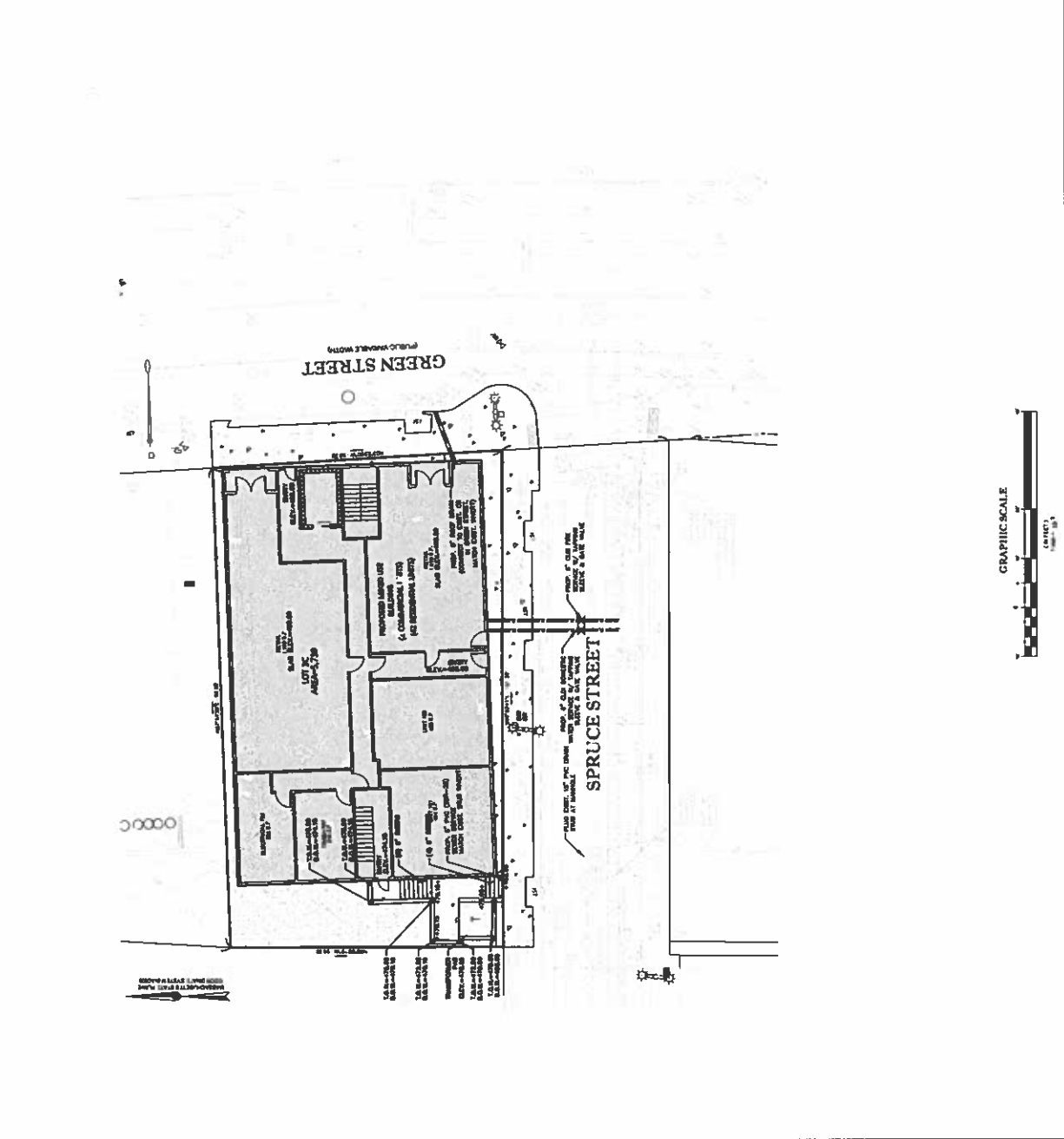
PROPERTY LINE	---
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EXISTING SEWER LINE	—S—
EXISTING GAS LINE	—G—
EXISTING ELECTRIC LINE	—E—
EXISTING TELEPHONE LINE	—T—
EXISTING STREET MANHOLE	◉
EXISTING CATCH BASIN	◉
EXISTING ELECTRIC MANHOLE	◉
EXISTING TELEPHONE MANHOLE	◉
EXISTING WATER GATE	◉
PROPOSED SPOT SHOT	•
PROPOSED SPARK LINE	- - -
PROPOSED ELECTRIC LINE	—E—
PROPOSED SEWER MANHOLE	◉
PROPOSED WATER LINE	—W—

GENERAL UTILITY NOTES:

1. ALL UTILITIES ARE LOCATED AS SHOWN ON THIS PLAN. THE LOCATION OF UTILITIES IS BASED ON THE RECORD DRAWINGS AND FIELD SURVEY DATA.
2. THE LOCATION OF UTILITIES IS SUBJECT TO CHANGE WITHOUT NOTICE.
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GENERAL DRAINAGE NOTES:

1. A MINIMUM OF 1% SLOPE IS REQUIRED FOR ALL DRAINAGE CHANNELS.
2. THE LOCATION OF DRAINAGE CHANNELS IS SUBJECT TO CHANGE WITHOUT NOTICE.
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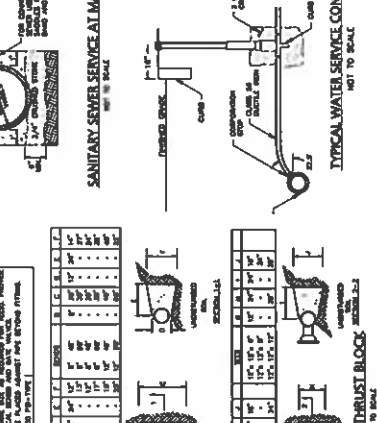
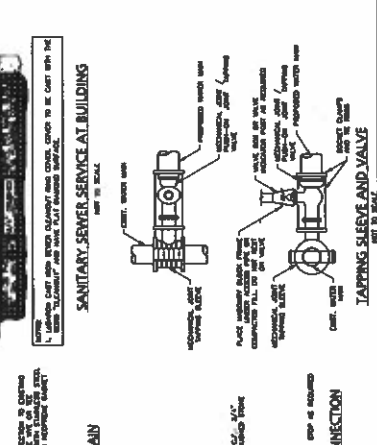
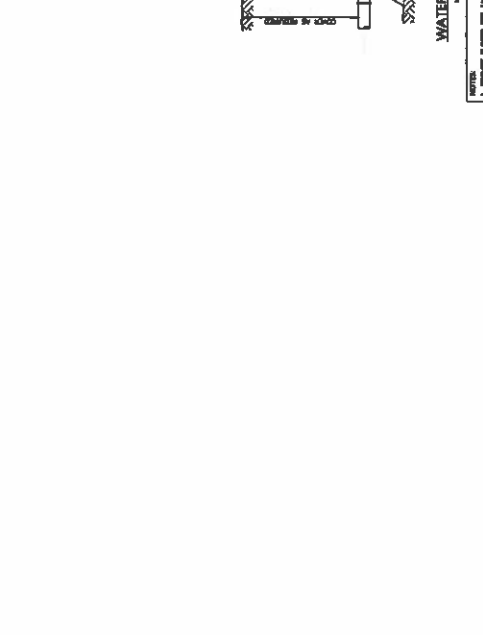
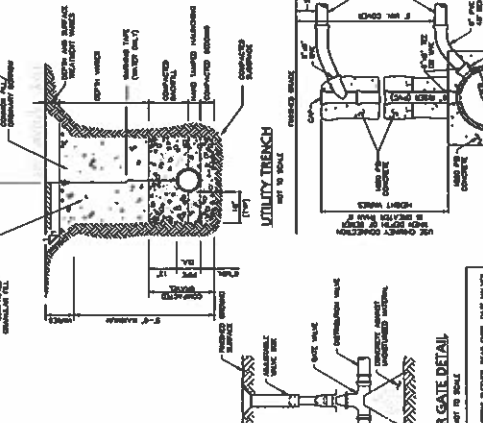
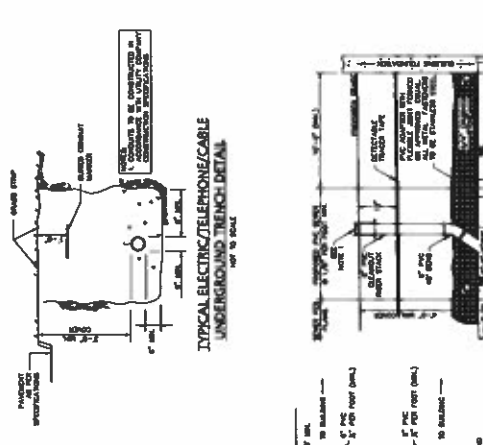
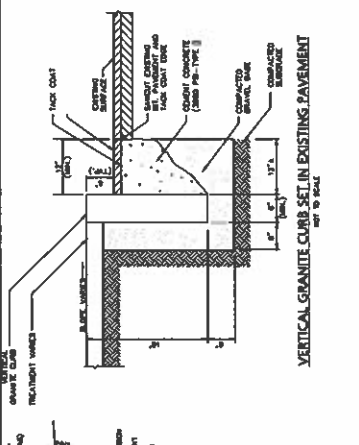
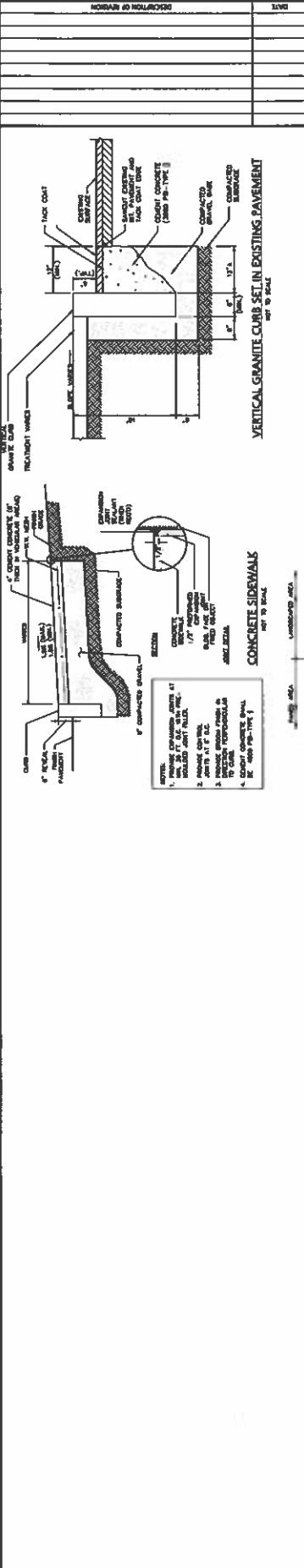
DESIGNED BY: JAC. BUCKENHAY, P.E.
 CONSULTING ENGINEER
 124 CANTON STREET
 NEEDHAM, MA 02459
 PHONE: (617) 552-2338
 FAX: (617) 552-2339
 PROJECT # 94-032

DATE: December 9, 2004
 DRAWING NO.: 24-032-D-01

PREPARED BY:
 Engineering Alliance, Inc.
 194 Central Street
 Needham, MA 02459
 Phone: (617) 417-0020
 Fax: (617) 417-1151

DESCRIPTION OF REVISION

DATE	DESCRIPTION OF REVISION



CONCRETE THRUST BLOCK
 NOT TO SCALE

SECTION 1
 SECTION 2
 SECTION 3

SECTION	SIZE	THICKNESS
1	12"	12"
2	12"	12"
3	12"	12"
4	12"	12"
5	12"	12"
6	12"	12"
7	12"	12"
8	12"	12"
9	12"	12"
10	12"	12"
11	12"	12"
12	12"	12"
13	12"	12"
14	12"	12"
15	12"	12"
16	12"	12"
17	12"	12"
18	12"	12"
19	12"	12"
20	12"	12"
21	12"	12"
22	12"	12"
23	12"	12"
24	12"	12"
25	12"	12"
26	12"	12"
27	12"	12"
28	12"	12"
29	12"	12"
30	12"	12"

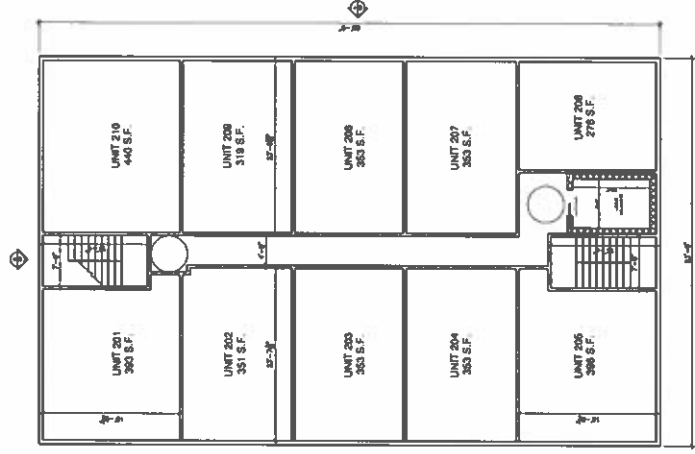
PAUL R. LESSARD
 • REGISTERED ARCHITECT •
 13 STATION ROAD SALEM, MA 01970
 (978) 210-1900 paul@paulrlessard.com

BUILDING LOT 3C
 GREEN STREET
 WORCESTER, MASSACHUSETTS

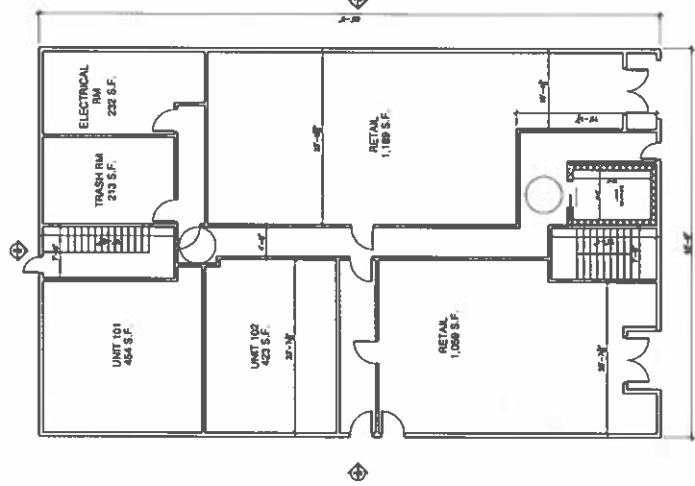
REVISIONS:

PROJECT: BUILDING LOT 3C
 DATE: 12/10/24
 SCALE: AS NOTED
 DRAWN: PRL
 CHECK:

DRAWING NUMBER:
A1



2 LOT 3C PROPOSED 2nd FLOOR PLAN
 1/8" = 1'-0"



1 LOT 3C PROPOSED 1st FLOOR PLAN
 1/8" = 1'-0"

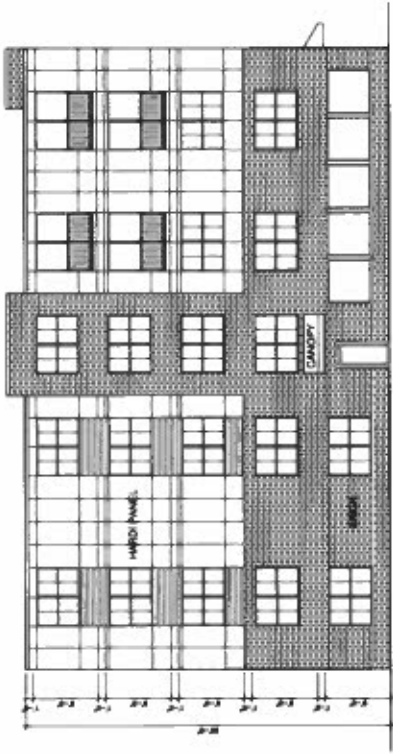
PAUL R. LESSARD
 • REGISTERED ARCHITECT
 13 STATION ROAD SALEM, MA 01970
 (978) 210-1960 paul@paulrlessard.com

BUILDING LOT 3C
 GREEN STREET
 WORCESTER, MASSACHUSETTS
 BUILDING 1X - PROPOSED EXTERIOR ELEVATIONS

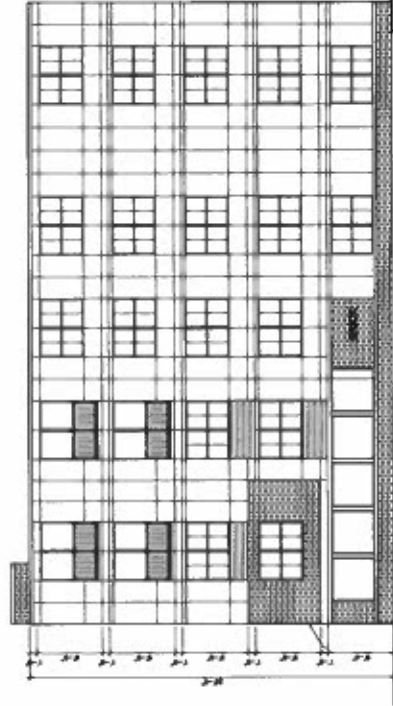
REVISIONS:

PROJECT NUMBER:
 DATE: 12/10/24
 SCALE: AS NOTED
 DRAWN BY: PRL
 CHECKED:

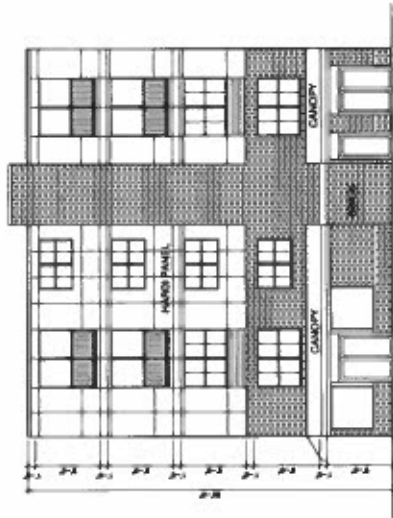
DRAWING NUMBER:
A2



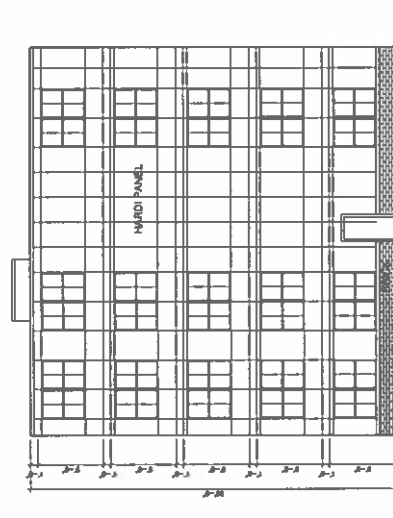
1 LOT 3C PROPOSED SOUTH ELEVATION
 1/8"=1'-0"



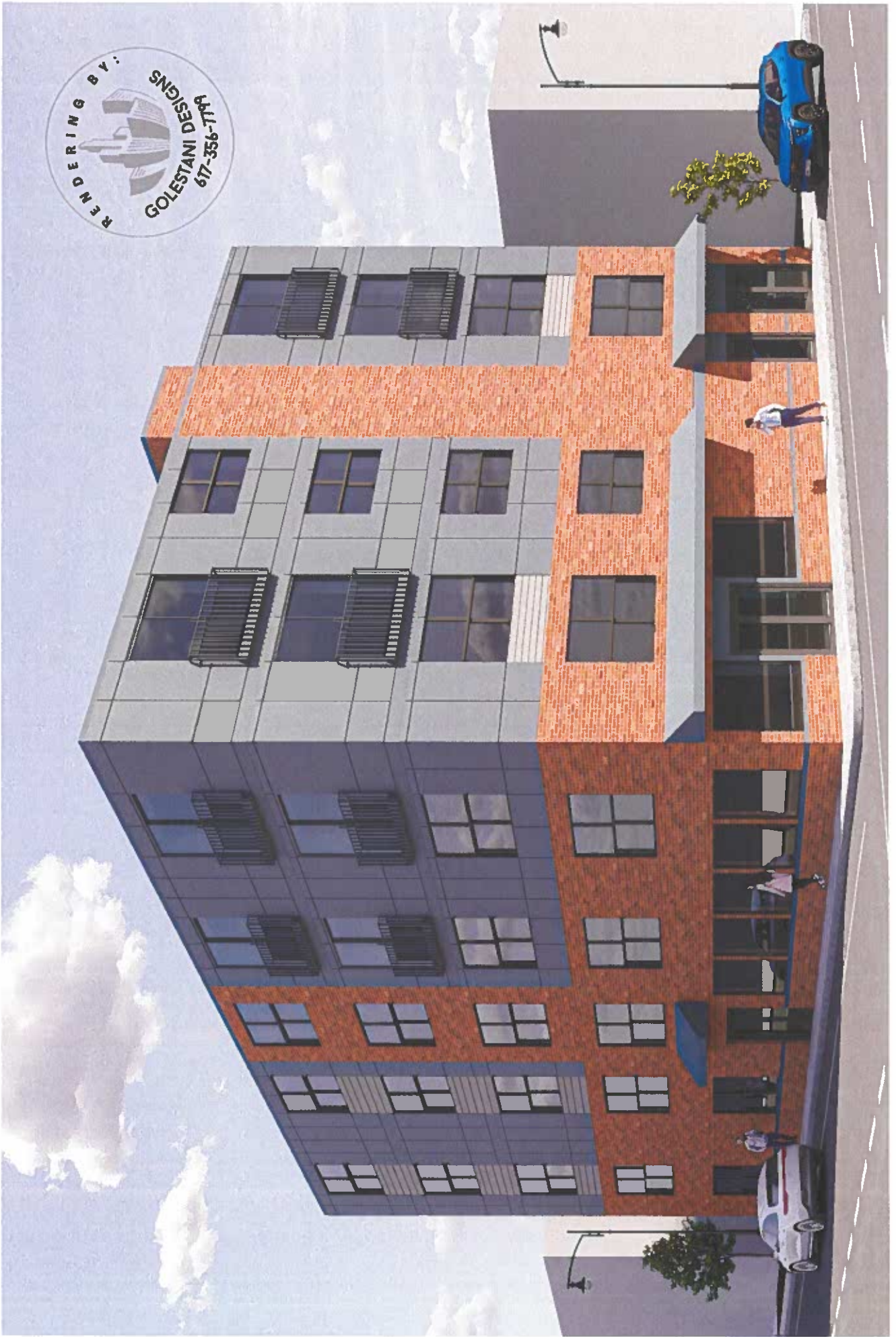
2 LOT 3C PROPOSED NORTH ELEVATION
 1/8"=1'-0"



3 LOT 3C PROPOSED EAST ELEVATION
 1/8"=1'-0"



4 LOT 3C PROPOSED WEST ELEVATION
 1/8"=1'-0"



Edward M. Augustus, Jr.
CITY MANAGER



Timothy J. McGourthy
CHIEF FINANCIAL OFFICER
Samuel E. Konieczny
CITY ASSESSOR

CITY OF WORCESTER
ADMINISTRATION & FINANCE

Certified Abutters List

A list of 'parties in interest' shall be attached to the application form and shall include the names and addresses. All such names and addresses shall be obtained from the most recent applicable tax list maintained by the City's Assessing Department. The Assessing Department certifies the list of names and addresses.

Total Count: 34

Parcel Address: 120 WASHINGTON ST
139 GREEN STREET
Assessor's Map-Block-Lot(s): 05-005-00001, 05-005-00015, 05-005-0002A
05-005-0003A, 05-005-0003B
05-005-0004A, 05-005-0005A

Owner: PARCEL 5 HOLDINGS LLC
C/O BOSTON CAPITAL DEVELOPMENT
11 BEACON ST SUITE 325
BOSTON, MA 02108

LAMEIMA LLC
36 VERNDALE ST
BROOKLINE, MA 02446

153 GREEN ST LLC
345 BOYLSTON ST SUITE 300
NEWTON, MA 02459

Owner Mailing: BOSTON CAPITAL DEVELOPMENT LLC
11 BEACON ST SUITE 325
BOSTON, MA 02108

Petitioner (if other than owner): MARK A BORENSTEIN
Petitioner Mailing Address: 120 FRONT ST SUITE 830
WORCESTER, MA 01608
508-688-9136

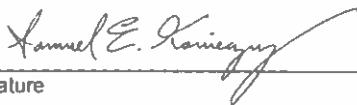
Planning: X Zoning: _____ Liquor License: _____ ConComm: _____
Historical: _____ Cannabis: _____ Other: _____

GOLDMAN, DANIEL H	05-004-15+24	0115 GREEN ST	WORCESTER, MA 01604-4123
DESJARDIN, SEAN TRUSTEE	04-021-19+25	0010 HARRISON ST	WORCESTER, MA 01604
ASH STREET, LLC	05-004-22+23	4 ASH STREET	WORCESTER, MA 01608
DESJARDIN, SEAN TRUSTEE	04-021-19+25	0010 HARRISON ST	WORCESTER, MA 01604
CANAL DISTRICT PARTNERS LLC	04-022-15+16	0004 ASH ST	WORCESTER, MA 01608
HESS RETAIL STORES LLC	05-008-08+09	539 SOUTH MAIN ST	FINDLAY, OH 45840
CANAL DISTRICT PARTNERS LLC	04-022-15+16	0004 ASH ST	WORCESTER, MA 01608

LORUSSO, LEONARD J	04-022-13+14	0015 HARRISON ST UNIT 6	WORCESTER, MA 01604
LORUSSO, LEONARD J	04-022-0001B	0015 HARRISON ST #6	WORCESTER, MA 01604
LORUSSO, LEONARD J	04-022-0001A	0015 HARRISON ST #6	WORCESTER, MA 01604
LORUSSO, LEONARD J	04-021-00020	0015 HARRISON ST UNIT 6	WORCESTER, MA 01604
139 GREEN STREET WORCESTER LLC	05-005-00015	0139 GREEN ST	WORCESTER, MA 01604
WHITE EAGLE ASSOCIATION OF	04-021-00009	0118 GREEN ST	WORCESTER, MA 01604
THE GOLD BLOCK REAL ESTATE LLC	05-004-00014	0175 PORTLAND ST FL 4	BOSTON, MA 02114
LORUSSO, LEONARD J	04-022-0001B	0015 HARRISON ST #6	WORCESTER, MA 01604
PRIFTI, NICHOLAS W + DINA C	04-019-00033	0431 ROSEDALE RD	AUBURN, MA 01501
ORMOND, MATTHEW J TRUSTEE	04-021-00022	0112 GREEN ST	WORCESTER, MA 01609
8 GOLD REAL ESTATE LLC	05-004-00017	0175 PORTLAND ST FL 4	BOSTON, MA 02114
FLETCHER, ALLEN W	05-004-00021	0004 ASH ST	WORCESTER, MA 01608
NISA INC	05-008-00002	0020 WESTWOOD DR	WORCESTER, MA 01609
FLETCHER, ALLEN W	05-004-00021	0004 ASH ST	WORCESTER, MA 01608
TRAN, Y THI	05-008-00011	0010 MILLBURY ST	WORCESTER, MA 01610
J + K GAS INC	05-008-00010	0072 SHREWSBURY ST STE 7	WORCESTER, MA 01604
TRAN, Y THI	05-008-00011	0010 MILLBURY ST	WORCESTER, MA 01610
K SQUARE REAL ESTATE LLC	05-008-00024	0175 PORTLAND ST FL 4	BOSTON, MA 02114
MADISON REALTY LLC	05-008-00027	2800 POST OAK BLVD STE 4200	HOUSTON, TX 77056
BAYSTATE INVESTMENT FUND LLC	04-022-00009	0100 GROVE ST	WORCESTER, MA 01605
BAYSTATE INVESTMENT FUND LLC	04-022-00009	0100 GROVE ST	WORCESTER, MA 01605
WORCESTER REDEVELOPMENT AUTHORITY	05-007-00004	0455 MAIN ST CITY HALL 4TH FLOOR	WORCESTER, MA 01608
BAYSTATE INVESTMENT LLC	04-022-00007	0100 GROVE ST	WORCESTER, MA 01605
PARCEL 5 HOLDINGS LLC	05-005-00001	11 BEACON ST SUITE 325	BOSTON, MA 02108
MADISON WG HOLDINGS LLC	05-010-00001	0667 BOYLSTON ST SUITE 201	BOSTON, MA 02116
MADISON WASHINGTON HOLDINGS LLC	05-010-0001H	667 BOYLSTON ST SUITE 201	BOSTON, MA 02116
MADISON WG HOLDINGS LLC	05-010-00001	0667 BOYLSTON ST SUITE 201	BOSTON, MA 02116

This is to certify that the above is a list of abutters to Assessor's Map-Block-Lot's 05-005-00001, 05-005-00015, 05-005-0002A, 05-005-0003A, 05-005-0003B, 05-005-0004A, 05-005-0005A as cited above.

Certified by:



Signature

11/08/2024
Date

Edward M. Augustus, Jr.
CITY MANAGER

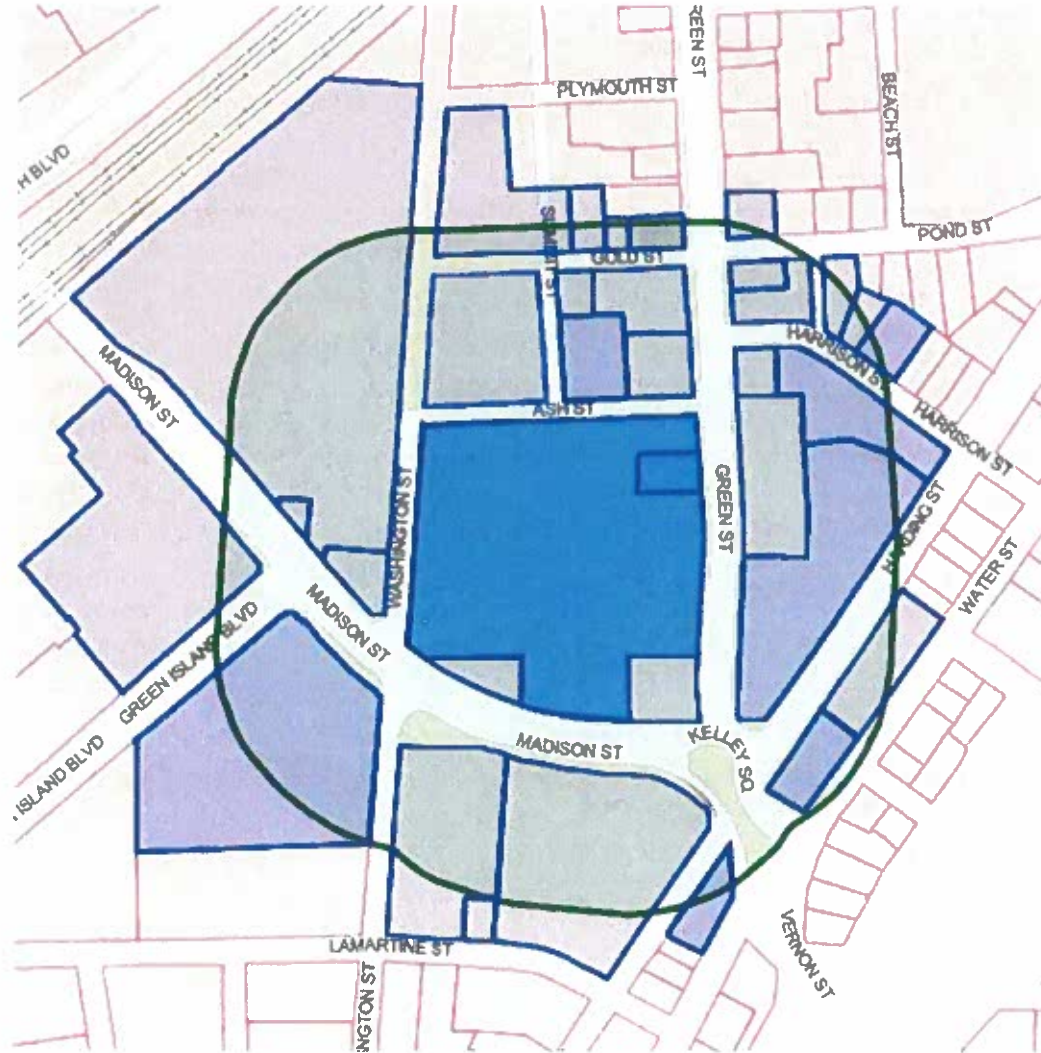


Timothy J. McGourthy
CHIEF FINANCIAL OFFICER

Samuel E. Konieczny
CITY ASSESSOR

CITY OF WORCESTER
ADMINISTRATION & FINANCE

Abutters Map



December 12, 2024

VIA EMAIL: PLANNING@WORCESTERMA.GOV

Worcester Planning Board
Division of Planning and Regulatory Services
City Hall, Room 404
455 Main Street
Worcester, MA 01608
Attn: Michelle Smith, Assistant Chief Development Officer



Re: *Rossi Development LLC – Application for Definitive Site Plan Approval for Mixed-Use Development at 120 Washington Street (Proposed Lot 3C), Worcester, MA (the “Property”)*

Dear Ms. Smith:

We represent Rossi Development LLC (the “Applicant”) in its applications to the City of Worcester Planning Board (the “Board”) for Definitive Site Plan Approval in connection with its construction and development of a new 5-story mixed-use building with 42 residential units, 2 commercial/retail ground floor units, landscaping and other site improvements related thereto at the Property. We hereby submit the following items for filing with the Board:

1. Zoning Determination Form (to be submitted under separate cover letter);
2. Preliminary Site Plan Approval Decision and Extension of Time Decision;
3. Application for Definitive Site Plan Approval (with Project Impact Statement and Statement in Support);
4. Aerial Photograph of the Property;
5. Civil Plan Set and Drainage Calculations and Stormwater Management Plan prepared by Engineering Alliance, Inc.;
6. Architectural Plans (including Floors Plans, Elevations and Rendering) prepared by Paul L. Lessard and Golestani Designs;
7. Traffic Memorandum prepared by The Engineering Corp.; and
8. Certified Abutters List.

We will coordinate with staff to facilitate the submission of the addressed envelopes and the filing fee to the City of Worcester.

Kindly file this application with the City Clerk, and schedule this application for a hearing at the Board's next available meeting, which is scheduled to occur on **January 15, 2025**.

Please contact me should have any questions regarding this application. Thank you.

Sincerely,



Mark A. Borenstein

Enclosures

cc: Project Team



DECISION – PRELIMINARY SITE PLAN

Application:	Preliminary Site Plan	File #:	PB-2023-008
Subject Property:	139 & 153 (aka 120 Washington Street) Green Street	Map Block Lot #:	05-005-00015 & 00001
Applicant(s):	Quarterra Multifamily Communities, LLC	Property Owner:	139 Green Street Worcester LLC & Parcel 5 Holdings LLC
Zoning District(s):	BG-6.0 & CCOD-D (Commercial Corridors Overlay District – Downtown Subarea)	Review Trigger:	Prelim
Existing:	Presently on premises of 139 Green Street is a single-story restaurant and 153 Green Street is a two-story commercial building.		
Proposed:	The applicant seeks to construct an 8 story 395,000SF mixed-use building with 22,000SF of commercial space on the ground floor, ± 375 dwelling units and a 7-story detached parking garage with ± 560 parking spaces.		
Plan Preparer:	Bohler Engineering	Plan Date:	dated 12/28/2022
Meeting date(s):	February 1, 2023	Board Action:	Approved 5-0 with modifications

Note: The approval of a preliminary plan does not alleviate the applicant from satisfactorily fulfilling any requirements of the Zoning Ordinance or Definitive Site Plan application as required in Article V, Section 4.B and Section 5 of the Zoning Ordinance, unless specifically waived.

Required Modifications:

The Definitive Site Plan submission shall include the following modifications:

1. Revised project impact statement and Definitive Site Plan application reflecting all changes.
2. Evidence of any recorded ANR plan or concurrent submission of an ANR plan creating the lots lines for Lot 3B as shown on the definitive site plan.
3. A plan reflecting the current property lines stamped and sealed by a MA Licensed PLS.
4. A zoning analysis table demonstrating zoning dimensional compliance, including building height in feet and compliant floor-area ratios for Lot 3B.
5. A Traffic Impact Study that meets the requirements of the City of Worcester's Guidelines for Performing Traffic Impact Studies.

Office of the City of Worcester Planning Board c/o Planning & Regulatory Services Division
City Hall, 455 Main Street, Room 404 (4th floor), Worcester, MA 01608

P | (508) 799-1400 x31440 F | (508) 799-1406 E | planning@worcesterma.gov W | www.worcesterma.gov/planning-regulatory

6. A parking analysis table demonstrating the breakdown (i.e. number and type of accessible, compact, & EV spaces) of the anticipated +/- 560 parking spaces by level, reflecting to-scale locations approximating any columns or supports to ensure appropriate stall spacing and adequacy of circulation and including a minimum of 15% EV or EV ready spaces (including accessible EV spaces).
7. A photometric plan demonstrating warm (less than 3000K), dark-sky compliant lighting, including any architectural or site lighting, and modeling any lights in the abutting rights-of-ways or driveways.
8. Approximate type, locations, and size of any proposed exterior signage.
9. Submission of to-scale architectural elevations of all building exteriors and floor plans for each story with all points of egress and overhangs (including balconies) labeled, consistent with the civil plans.
 - a. Reflect use of a white or reflective roof treatment on the mixed-use building.
 - b. Reflect active façades (further detailing the submitted renderings) in accordance with the guidelines of the CCOD (Article IX, Section 6) and following the City of Worcester Urban Design Guidelines (November 2012) to the maximum extent feasible.
10. Revised renderings reflecting the exterior elevations.
11. Details and materials plan for any walls including top and bottom elevations at all endpoints, highest points, and points of intersection including any wall drainage components (weep holes, drainage connections, etc.).
12. A planting plan and schedule including location, species and caliper size of all plantings and reflecting no less than 4 different tree species, in accordance with the City's current planting recommendations. Providing details for tree protection and plant installation as well as specifications for any hardscaping and final surface treatments.
13. Subject to obtaining necessary permits and approvals, the "Spruce Street" streetscape shall reflect:
 - a. 10' of sidewalk or hardscape between the building face and edge of the sidewalk curbing.
 - b. With regard to the garage entry:
 - i. Curb cut shall be the minimum feasible width.
 - ii. Sidewalk shall continue at-grade through the curb-cut.
 - iii. The entry gate locations shall be specified and set back within the garage such that a vehicle waiting to enter is not blocking the sidewalk or bicycle lane.
 - iv. The applicant shall incorporate a pedestrian/bicyclist warning system for cars egressing the parking garage.
 - c. Pedestrian entrances to retail/commercial use shall be accentuated by the architecture and important interior spaces (e.g. lobbies or retail uses) shall be visible from the street.
 - d. Street tree locations shall not conflict with any proposed doors.
14. The Green Street streetscape shall reflect:
 - a. Closure of existing curb cut near the intersection with Ash Street.
 - b. Pedestrian amenities as depicted in renderings provided, including but not limited to landscaping, street trees, and seating areas.
 - c. Consider further modulation to the facade
15. The Ash Street streetscape shall reflect:

- a. Changes to the mixed-use building footprint to accommodate circumnavigation of the site on a separated pedestrian facility and a pedestrian access near the intersection of Ash & Washington.
 - b. 6-8' of sidewalk or pedestrian space between building and edge of sidewalk curb along the southern side of Ash Street between Green Street and Washington Street.
 - c. The applicant shall coordinate with DPW & DTM to remove the diagonal crosswalk at the intersection of Ash Street and Summit Street and replace it with a perpendicular crosswalk at an appropriate, safe location.
 - d. The private amenity spaces shall incorporate appropriate design elements along Ash Street, using open style materials and buffers where possible to avoid detriment to the pedestrian environment.
 - e. Reflect closure of existing curb cuts.
 - f. Incorporation of pedestrian access along Ash Street in at least 3 locations (e.g. mixed-use building exits and/or cross walks from Green Street and Washington Street).
 - g. 10' of sidewalk or pedestrian space between building and edge of sidewalk curb at the intersection of southern side of Ash Street and along the eastern side of Washington Street.
16. The Washington Street streetscape shall reflect:
- a. 10' of sidewalk or pedestrian space between building and edge of sidewalk curb.
17. The parking garage and Madison Street streetscape shall reflect:
- a. Compliance with setbacks and landscaping requirements relative to the parking garage and the Madison and Spruce Street lot lines, as described in Article IX, Section 6 A & F. The applicant is encouraged to evaluate setbacks along Spruce Street to improve sightlines for exiting vehicles and provide a buffer to the sidewalk.
 - b. Reflect closure of all existing curb cuts on Madison Street except those required for vehicular access to/from the parking garage, which shall be the minimum width feasible.
 - c. Any curb cuts required for access to the parking garage shall be designed to prioritize pedestrians and bicyclists to the extent feasible and consolidated with neighboring site where appropriate and possible.
 - d. Demonstrate compliance with Article IX, Section 6B, with façade details, pedestrian entrances, and landscaping to maintain an attractive streetscape.
 - e. If an active use cannot feasibly be located on the ground floor fronting Madison Street, the definitive submission shall include an explanation of why this is infeasible and what alternatives were specifically explored.
 - f. Reflect pedestrian circulation, separated from vehicles, at the ground level.
18. Utility plans for gas, HVAC, and electric should reflect site infrastructure with connections and transformers away from the street to the maximum extent practical. Backflow preventers shall be evaluated for all sewerage connections.
19. A construction period erosion and sediment control plan, including proposed dust control measures and any plans for dewatering.
20. A complete stormwater report and checklist stamped and sealed by a MA licensed PE with a detailed narrative for each of the 10 standards documenting compliance with the MA Stormwater Standards and Handbook, all calculations shall be performed using NOAA Atlas 14 precipitation data. Actual soil test pit information shall be provided within the locations of any proposed infiltration BMPs including assumptions for groundwater elevations -infiltration

of roof-runoff should be fully investigated for the three courtyard areas. Given the extent of redevelopment TSS removal rates should meet the 80% removal standard. Any opportunities to connect with surface drainage system infrastructure shall be evaluated and prioritized.

21. A construction period staging and circulation management plan (including temporary fencing, loading/unloading and parking or shuttling for workers, sidewalks to be obstructed, etc.).
22. The applicant shall either remove the skybridge from architectural renderings or reflect the bridge on the civil drawings and demonstrate compliance with the Worcester Zoning Ordinance. If retained, details on minimum vertical clearance (the distance for which must minimally enable a tractor trailer and the city's largest fire apparatus to drive-under without issue) and materials shall be provided in to-scale elevations.
23. Submitted plans shall comply with all the plan requirements outlined in the City of Worcester's Zoning Ordinance and the Planning Board's Rules & Regulations for Definitive Site Plans.
24. The applicant shall provide any materials that the Board deems necessary in order to grant Definitive Site Plan approval and shall demonstrate compliance with all applicable provisions of the City of Worcester's Zoning Ordinance.

Waivers Granted

No waivers from the plan requirements for Definitive Site Plan approval are conferred.

Authorized Signature,

DocuSigned by:

Michelle Smith

26350A8C7CF44D3

DATE 03/10/2023

Michelle Smith, *Assistant Chief Development Officer*
On behalf of the Worcester Planning Board

REMINDERS

Time Limitations: Approval under this article shall become invalid unless the work or action authorized by it shall commence within one year after the Planning Board has granted such approval and thereafter shall proceed in good faith continuously to completion so far as is reasonably practicable under the circumstances. Per Article V, Section 3F "Site Plan Review" of the City of Worcester Zoning Ordinance.

Landscaping Requirements: Required landscaping shall be maintained in a healthy growing condition, free of refuse and debris, and any plantings that do not survive shall be replaced in kind by the applicant or the property owner within a reasonable period of time. All plant materials and fencing shall be arranged and maintained so as not to obscure the vision of traffic. Per Article V, Section 5, C. iv of the City of Worcester Zoning Ordinance.

Construction Noise: No person shall operate any powered construction equipment or build, erect construct, demolish, alter, repair, excavate or engage in hoisting, grading, site work, including tree and brush removal, dredging or pneumatic hammering, or deliver construction equipment and/or supplies to the site on any building, road, tower, parking lot, machine, pipe, sewer, sidewalk, or any other construction project, except between the hours of 7:00 a.m. and 9:00 p.m. on weekdays and Saturday, and between the hours of 9:00 a.m. and 7:00 p.m. on Sundays....Per Chapter 9 Section 1A (e) (9) of the City of Worcester's Ordinance Relative to Excessive and Unreasonable Noise.



The City of
WORCESTER

Planning Board

Albert LaValley, Chair
Edward Moynihan, Vice Chair
Adrian Angus
Conor McCormack
Brandon King

DECISION – PRELIMINARY SITE PLAN – EXTENSION OF TIME

Application:	Preliminary Site Plan Extension of Time	File #:	PB-2024-008
Subject Property:	139 & 153 (aka 120 Washington Street) Green Street	Map Block Lot #:	05-005-00015 & 00001
Applicant(s):	Boston Capital Development, LLC	Property Owner:	Boston Capital Development, LLC & 139 Green Street Worcester, LLC
Zoning District(s):	BG-6.0 & CCOD-D (Commercial Corridors Overlay District – Downtown Subarea)	Review Trigger:	Preliminary
Existing:	Presently on premises of 139 Green Street is a single-story restaurant and 153 Green Street is a two-story commercial building.		
Proposed:	The applicant seeks to construct an 8 story 395,000SF mixed-use building with 22,000SF of commercial space on the ground floor, ± 375 dwelling units and a 7-story detached parking garage with ± 560 parking spaces.		
Plan Preparer:	Bohler Engineering	Plan Date:	dated 12/28/2022
Meeting date(s):	February 28, 2024	Board Action:	Approved 5-0 with modifications

The Planning Board approves the Preliminary Site Plan Extension of Time. The Extension of Time is granted for a period of one year to expire on February 1, 2025. The Board approves the Extension of Time subject to the original modifications, listed below:

Note: The approval of a preliminary plan does not alleviate the applicant from satisfactorily fulfilling any requirements of the Zoning Ordinance or Definitive Site Plan application as required in Article V, Section 4.B and Section 5 of the Zoning Ordinance, unless specifically waived.

Required Modifications:

The Definitive Site Plan submission shall include the following modifications:

1. Revised project impact statement and Definitive Site Plan application reflecting all changes.
2. Evidence of any recorded ANR plan or concurrent submission of an ANR plan creating the lots lines for Lot 3B as shown on the definitive site plan.
3. A plan reflecting the current property lines stamped and sealed by a MA Licensed PLS.

Office of the City of Worcester Planning Board c/o Planning & Regulatory Services Division
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4. A zoning analysis table demonstrating zoning dimensional compliance, including building height in feet and compliant floor-area ratios for Lot 3B.
5. A Traffic Impact Study that meets the requirements of the City of Worcester's Guidelines for Performing Traffic Impact Studies.
6. A parking analysis table demonstrating the breakdown (i.e. number and type of accessible, compact, & EV spaces) of the anticipated +/- 560 parking spaces by level, reflecting to-scale locations approximating any columns or supports to ensure appropriate stall spacing and adequacy of circulation and including a minimum of 15% EV or EV ready spaces (including accessible EV spaces).
7. A photometric plan demonstrating warm (less than 3000K), dark-sky compliant lighting, including any architectural or site lighting, and modeling any lights in the abutting rights-of-ways or driveways.
8. Approximate type, locations, and size of any proposed exterior signage.
9. Submission of to-scale architectural elevations of all building exteriors and floor plans for each story with all points of egress and overhangs (including balconies) labeled, consistent with the civil plans.
 - a. Reflect use of a white or reflective roof treatment on the mixed-use building.
 - b. Reflect active façades (further detailing the submitted renderings) in accordance with the guidelines of the CCOD (Article IX, Section 6) and following the City of Worcester Urban Design Guidelines (November 2012) to the maximum extent feasible.
10. Revised renderings reflecting the exterior elevations.
11. Details and materials plan for any walls including top and bottom elevations at all endpoints, highest points, and points of intersection including any wall drainage components (weep holes, drainage connections, etc.).
12. A planting plan and schedule including location, species and caliper size of all plantings and reflecting no less than 4 different tree species, in accordance with the City's current planting recommendations. Providing details for tree protection and plant installation as well as specifications for any hardscaping and final surface treatments.
13. Subject to obtaining necessary permits and approvals, the "Spruce Street" streetscape shall reflect:
 - a. 10' of sidewalk or hardscape between the building face and edge of the sidewalk curbing.
 - b. With regard to the garage entry:
 - i. Curb cut shall be the minimum feasible width.
 - ii. Sidewalk shall continue at-grade through the curb-cut.
 - iii. The entry gate locations shall be specified and set back within the garage such that a vehicle waiting to enter is not blocking the sidewalk or bicycle lane.
 - iv. The applicant shall incorporate a pedestrian/bicyclist warning system for cars egressing the parking garage.
 - c. Pedestrian entrances to retail/commercial use shall be accentuated by the architecture and important interior spaces (e.g. lobbies or retail uses) shall be visible from the street.
 - d. Street tree locations shall not conflict with any proposed doors.
14. The Green Street streetscape shall reflect:
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 - c. Consider further modulation to the facade
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 - c. The applicant shall coordinate with DPW & DTM to remove the diagonal crosswalk at the intersection of Ash Street and Summit Street and replace it with a perpendicular crosswalk at an appropriate, safe location.
 - d. The private amenity spaces shall incorporate appropriate design elements along Ash Street, using open style materials and buffers where possible to avoid detriment to the pedestrian environment.
 - e. Reflect closure of existing curb cuts.
 - f. Incorporation of pedestrian access along Ash Street in at least 3 locations (e.g. mixed-use building exits and/or cross walks from Green Street and Washington Street).
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- a. 10' of sidewalk or pedestrian space between building and edge of sidewalk curb.
17. The parking garage and Madison Street streetscape shall reflect:
- a. Compliance with setbacks and landscaping requirements relative to the parking garage and the Madison and Spruce Street lot lines, as described in Article IX, Section 6 A & F. The applicant is encouraged to evaluate setbacks along Spruce Street to improve sightlines for exiting vehicles and provide a buffer to the sidewalk.
 - b. Reflect closure of all existing curb cuts on Madison Street except those required for vehicular access to/from the parking garage, which shall be the minimum width feasible.
 - c. Any curb cuts required for access to the parking garage shall be designed to prioritize pedestrians and bicyclists to the extent feasible and consolidated with neighboring site where appropriate and possible.
 - d. Demonstrate compliance with Article IX, Section 6B, with façade details, pedestrian entrances, and landscaping to maintain an attractive streetscape.
 - e. If an active use cannot feasibly be located on the ground floor fronting Madison Street, the definitive submission shall include an explanation of why this is infeasible and what alternatives were specifically explored.
 - f. Reflect pedestrian circulation, separated from vehicles, at the ground level.
18. Utility plans for gas, HVAC, and electric should reflect site infrastructure with connections and transformers away from the street to the maximum extent practical. Backflow preventers shall be evaluated for all sewerage connections.
19. A construction period erosion and sediment control plan, including proposed dust control measures and any plans for dewatering.
20. A complete stormwater report and checklist stamped and sealed by a MA licensed PE with a detailed narrative for each of the 10 standards documenting compliance with the MA Stormwater Standards and Handbook, all calculations shall be performed using NOAA Atlas 14 precipitation data. Actual soil test pit information shall be provided within the locations of any proposed infiltration BMPs including assumptions for groundwater elevations -infiltration of roof-runoff should be fully investigated for the three courtyard areas. Given the extent of redevelopment TSS removal rates should meet the 80% removal standard. Any opportunities to connect with surface drainage system infrastructure shall be evaluated and prioritized.

- 21. A construction period staging and circulation management plan (including temporary fencing, loading/unloading and parking or shuttling for workers, sidewalks to be obstructed, etc.).
- 22. The applicant shall either remove the skybridge from architectural renderings or reflect the bridge on the civil drawings and demonstrate compliance with the Worcester Zoning Ordinance. If retained, details on minimum vertical clearance (the distance for which must minimally enable a tractor trailer and the city's largest fire apparatus to drive-under without issue) and materials shall be provided in to-scale elevations.
- 23. Submitted plans shall comply with all the plan requirements outlined in the City of Worcester's Zoning Ordinance and the Planning Board's Rules & Regulations for Definitive Site Plans.
- 24. The applicant shall provide any materials that the Board deems necessary in order to grant Definitive Site Plan approval and shall demonstrate compliance with all applicable provisions of the City of Worcester's Zoning Ordinance.

Waivers Granted

No waivers from the plan requirements for Definitive Site Plan approval are conferred.

Authorized Signature,

DocuSigned by:

 20366A8970F44D8

DATE 05/31/2024

Michelle Smith, *Assistant Chief Development Officer*
On behalf of the Worcester Planning Board

REMINDERS

Time Limitations: Approval under this article shall become invalid unless the work or action authorized by it shall commence within one year after the Planning Board has granted such approval and thereafter shall proceed in good faith continuously to completion so far as is reasonably practicable under the circumstances. Per Article V, Section 3F "Site Plan Review" of the City of Worcester Zoning Ordinance.

Landscaping Requirements: Required landscaping shall be maintained in a healthy growing condition, free of refuse and debris, and any plantings that do not survive shall be replaced in kind by the applicant or the property owner within a reasonable period of time. All plant materials and fencing shall be arranged and maintained so as not to obscure the vision of traffic. Per Article V, Section 5, C. iv of the City of Worcester Zoning Ordinance.

Construction Noise: No person shall operate any powered construction equipment or build, erect construct, demolish, alter, repair, excavate or engage in hoisting, grading, site work, including tree and brush removal, dredging or pneumatic hammering, or deliver construction equipment and/or supplies to the site on any building, road, tower, parking lot, machine, pipe, sewer, sidewalk, or any other construction project, except between the hours of 7:00 a.m. and 9:00 p.m. on weekdays and Saturday, and between the hours of 9:00 a.m. and 7:00 p.m. on Sundays....Per Chapter 9 Section 1A (e) (9) of the City of Worcester's Ordinance Relative to Excessive and Unreasonable Noise.